

3. Project Description

This chapter of the Draft Environmental Impact Report (EIR) describes the proposed Millbrae Station Area Specific Plan Update (Specific Plan Update) and associated amendments to the General Plan and Zoning Ordinance, and the proposed Transit-Oriented Developments (TOD) #1 and #2 (together referred to as the “proposed Project”).¹ The proposed Specific Plan Update would guide future development in the Specific Plan Area, including the two proposed TOD projects.

This Project Description provides a general overview of the proposed Project, including the background and planning process for the Specific Plan Update and provides detailed descriptions of the proposed Specific Plan Update and the proposed TOD #1 and #2 projects.

3.1 OVERVIEW

3.1.1 BACKGROUND

Millbrae is largely a single-family residential community with a population of just over 22,000 people. Over the years, Millbrae has transformed from a “rolling hills” estate to a growing community driven by transportation initiatives. From the advent of the #40 “interurban” streetcar at the onset of the 20th century to the opening of the San Francisco International Airport (SFO) in the late 1920s, transportation has been integral in shaping the city’s growth.

The Millbrae Bay Area Rapid Transit (BART)/Caltrain Station (Millbrae Station) opened in June 2003 as part of the SFO extension. The Millbrae Station, the largest intermodal terminal in the western United States, is a key intermodal connection between BART and Caltrain’s commuter rail systems near SFO. In advance of the Millbrae Station’s opening, the City of Millbrae (City) developed the Millbrae Station Area Specific Plan in 1998 for the 116-acre area (inclusive of existing roadways). The 1998 Millbrae Station Area Specific Plan as amended by the City Council in 2002 (1998 Specific Plan), identified a vision for the area around the Millbrae Station, set development standards for new buildings, and implementation strategies for 13 sub-areas in the Specific Plan Area.

While some development has successfully occurred under the 1998 Specific Plan, new development applications, including the proposed TOD #1 (sub-area 1) and TOD #2 (sub-areas 5, 6 and 8) projects, and economic shifts have created a need to update the 1998 Specific Plan. For these reasons, the City decided to embark on an effort to

¹ See Sections 3.2.8, 3.3.7, and 3.4.7, Required Approvals, of this chapter, for a discussion on the required approvals for each Project component.

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update the 1998 Specific Plan with the overall purpose of the Specific Plan Update being to refine the vision for the Specific Plan Area and multimodal and circulation improvements, as well as other infrastructure improvements required to accommodate new development.

3.1.2 PLANNING PROCESS

The City has undertaken a community-based planning process to review land use alternatives as part the proposed Specific Plan Update. The planning process for the proposed Specific Plan Update began in early 2014 with the preparation of technical studies and a market assessment. A community workshop was held on May 15, 2014 to introduce the planning process and solicit community feedback on issues and opportunities for the proposed Specific Plan Update. The first community workshop included an overview of the 1998 Specific Plan, existing Specific Plan Area conditions, and market assessment findings. The workshop established key themes related to traffic and safety, the need for better bicycle and pedestrian connections, economic development opportunities, revenue-generating uses and businesses, and land use variety.

Two conceptual alternatives were developed based on the issues and opportunities discussed at the first community workshop. The first alternative included a corridor focus and emphasized new development oriented toward major streets, bicycle and pedestrian circulation along major streets, linear open space, and a new employment center. The second alternative included an internal focus and emphasized new development concentrated on interior paths and open space, bicycle and pedestrian paths intertwined through new development, nodal open spaces, a new residential node, a new Class A² office/research and development (R&D) center, and expansion of the existing retail area. The two alternatives were presented at the June 26, 2014 community workshop. Workshop participants generally supported an alternative that would focus bicycle and pedestrian circulation on smaller streets, retain Millbrae's small-town character, consider a wide variety of uses for the Specific Plan Area, establish gateways at key locations, create safer connections to Downtown, and minimize impacts to nearby residential areas.

Based on the input received at the second community workshop, a single draft alternative was developed and subsequently presented at the July 22, 2014 joint Planning Commission/City Council meeting. The draft alternative included a wide variety of uses in areas closest to the Millbrae Station, with a Class A office corridor south of Adrian Road, a mixed-use corridor along El Camino Real, and gateways to accentuate key entries into Millbrae. The draft alternative prioritized bicycle circulation along smaller interior streets, included a connection to the planned Bay Trail, and prioritized pedestrian circulation along all streets, with new connections through development projects and enhancements at major intersections.

² These buildings represent the highest quality buildings in their market. They are generally the best looking buildings with the best construction, and possess high quality building infrastructure. Class A buildings also are well-located, have good access, and are professionally managed. As a result, they attract the highest quality tenants and also command the highest rents.

As part of the planning process, a Technical Advisory Committee (TAC) was established that included representatives from the local development community and the following agencies:

- City of Millbrae (City)
- Bay Area Rapid Transit (BART)
- San Mateo County Transit District (SamTrans)
- San Francisco International Airport (SFO)
- City/County Association of Governments of San Mateo County (C/CAG)
- Federal Aviation Administration (FAA)
- California Department of Transportation (Caltrans)
- California High Speed Rail Authority (CAHSRA)

The TAC met three times during the planning process to identify critical issues, review technical studies and the market assessment, and review the preliminary alternative. Following the joint Planning Commission/City Council meeting and third TAC meeting, a draft alternative was selected for inclusion in the proposed Specific Plan Update. This alternative, which is described in detail below in Section 3.2, Description of the Proposed Specific Plan Update, is the subject of this Draft EIR.

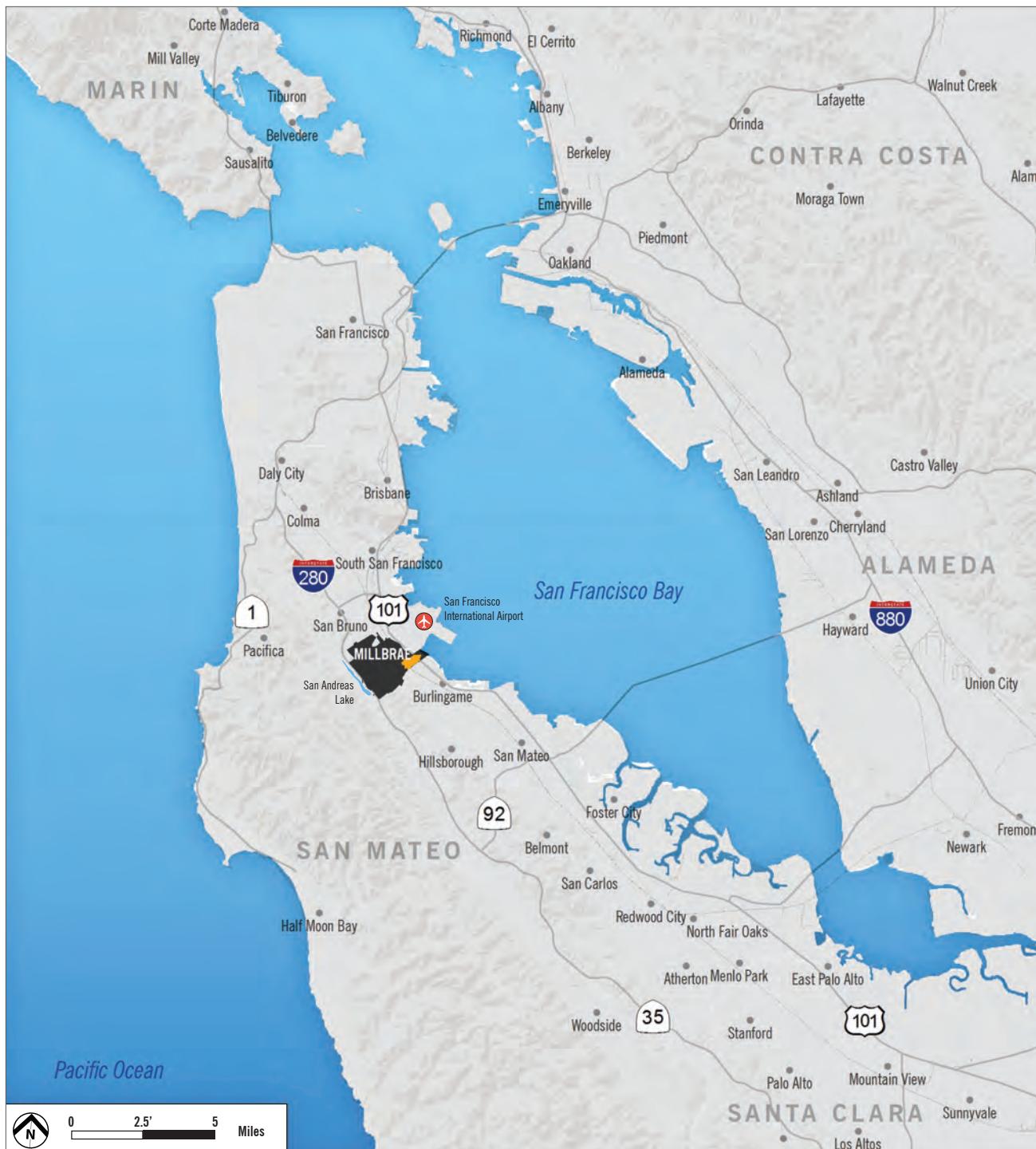
3.1.2.1 HIGH SPEED RAIL

The CAHSRA is currently undergoing a separate planning process for the High Speed Rail (HSR), a state-of-the-art-train system that will run from San Francisco to Los Angeles under Phase One, and is proposed to extend to both Sacramento and San Diego under Phase Two. The HSR system is divided into rail “segments,” with the city being located within the San Jose to San Francisco segment. A draft report showing analysis of various options of track alignments for shaping the San Francisco to San Jose segment was released on April 8, 2010.³ The Specific Plan Update has been drafted to consider the HSR project; however, the HSR project is not evaluated in this EIR.

3.1.3 REGIONAL LOCATION

The city is a 3.25 square-mile city within San Mateo County located approximately 14 miles south of central San Francisco and 30 miles north of the City of San Jose via Highway 101. As shown on Figure 3-1, Millbrae is located on the San Francisco peninsula, bordered by the City of San Bruno to the north, SFO and the San Francisco Bay to the east, the City of Burlingame to the south, and the San Andreas Lake and Interstate 280 to the west. Millbrae is accessed by Interstate 280 and Highway 101, which function as the main north-south traffic routes in the region. El Camino Real (Highway 82) also provides a regional connection through Millbrae. The Specific Plan Area is located in the southeast corner of the city.

³California High Speed Rail Authority website,
http://www.hsr.ca.gov/docs/about/business_plans/BPlan_2014_Business_Plan_Final.pdf accessed April 26, 2015.



Source: PlaceWorks, 2015.

 City of Millbrae

 Millbrae Station Area Specific Plan Area

Figure 3-1
Regional Location Map

3.1.4 INTENDED USES OF THE EIR

This EIR is intended to disclose and assess potential environmental impacts associated with the adoption and implementation of the proposed Specific Plan Update and associated General Plan and Zoning Ordinance Amendments, and the proposed TOD #1 and TOD #2 projects, and to determine corresponding mitigation measures, as necessary. While this EIR serves as a single CEQA document for the proposed Specific Plan Update and two proposed TOD projects, the proposed Specific Plan Update and each proposed TOD project will be considered separately by the City for project approval. See Sections 3.2.8, 3.3.7, and 3.4.7, Required Approvals, of this chapter, for a discussion on the required approvals for each Project component.

This EIR provides a project-level review of the proposed TOD #1 and TOD #2 projects and does not evaluate the impacts of other future specific, individual developments that may be allowed under the program-level review of the proposed Specific Plan Update. Each future project outside the TOD project sites would require environmental review, as required by CEQA, to secure the necessary discretionary development permits.

Therefore, while subsequent environmental review may be tiered off the program-level review in this EIR, this EIR is not intended to address impacts of future individual projects. Subsequent projects will be reviewed by the City for consistency with the proposed Specific Plan Update, General Plan, Zoning Ordinance, and this EIR, and subsequent project-level environmental review will be conducted as required by CEQA. Projects successive to this EIR may include, but are not limited to, the following:

- Approval and funding of major public projects and capital improvements.
- Issuance of permits and other approvals necessary for implementation of the proposed Specific Plan Update.
- Development plan approvals, such as tentative maps, variances, conditional use permits, and other land use permits.
- Permit issuances and other approvals necessary for public and private development projects.
- Development agreement processes and approvals.

See Chapter 1, Introduction, Section 1.3, Type of EIR, of this Draft EIR for a detailed discussion on the environmental review applied in this EIR.

3.1.5 SUMMARY OF TOTAL BUILDOUT PROJECTIONS

Section 15126.2 of the California Environmental Quality Act (CEQA) Guidelines requires that an EIR focus on the significant “direct and indirect” and “short-term and long-term” effects of a project. To ensure a conservative approach in analyzing environmental effects under CEQA, EIRs typically analyze what could be considered a worst-case scenario in order to capture as many significant environmental effects as could reasonably be expected as a result of the project. For a programmatic evaluation of a land use plan, this entails projecting buildout calculations to carry through the environmental review process.

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The buildout projections shown in Table 3-1 reflect the estimated number of office and retail development, housing units and hotel rooms, and resident and employment populations that are reasonably foreseeable for the 25-year duration of the proposed Specific Plan Update and for the two proposed TOD projects shown on Figure 3-2. The actual rate and amount of development for the areas outside the TOD project sites will be dependent on market conditions and regulatory processes.

TABLE 3-1 PROPOSED PROJECT BUILDOUT PROJECTIONS SUMMARY

	Office SF	Retail SF	Industrial /Non-Retail ^a SF	Residential Units ^b	Hotel Rooms	Permanent Population ^c	Employees ^d
Existing Specific Plan							
Total Existing ^e	76,100	132,575	335,240	308	39	816	1,002
Specific Plan Update							
TOD #1 Project	267,000	32,000	0	500	0	1,325	1,148
TOD #2 Project	164,535	46,935	0	321	116	851	868
Remaining Specific Plan Area ^f	1,213,300	101,700	0	617	124	1,635	5,207
<i>Total Net Increase^g</i>	<i>1,577,235</i>	<i>142,535</i>	<i>-335,240</i>	<i>1,440</i>	<i>325</i>	<i>3,808</i>	<i>6,590</i>
<i>Total Buildout^h</i>	<i>1,653,340</i>	<i>275,110</i>	<i>0</i>	<i>1,750</i>	<i>370</i>	<i>4,640</i>	<i>7,600</i>

Notes: SF = square feet, TOD = transit-oriented development

a. The proposed Project does not include Industrial/Non-Retail land uses.

b. The proposed residential development would be multi-family units.

c. Population is based on 2.65 persons per dwelling units consistent with U.S. Census Bureau's 2005-2009 American Community Survey 5 year estimates. Temporary residents associated with the hotel, not shown on this table, are estimated at an average of 2 persons per room as part of the environmental review for this Draft EIR.

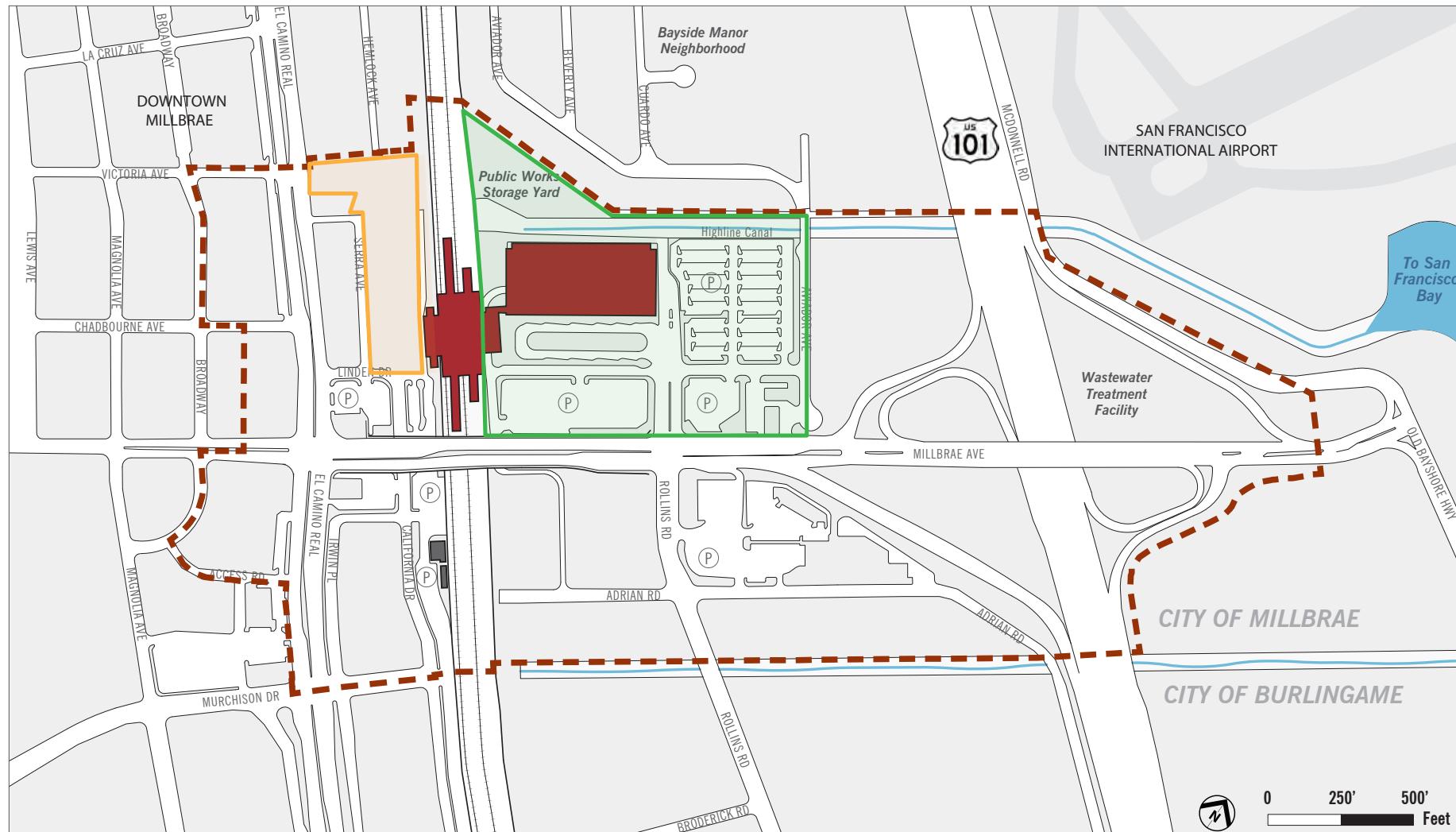
d. Jobs are calculated by applying 1 job/250 sf for office; 1 job/400 sf for retail; 1 job/1,000 sf industrial/non-retail; and 1 job per 1.25 hotel rooms.

e. The total existing represents what is currently developed (i.e. built on the ground).

f. The "Remaining Specific Plan Area" includes the projected buildout excluding the TOD #1 and #2 project sites.

g. The total net increase represents the amount of new development beyond what is currently developed and what is proposed to be redeveloped under the Specific Plan Update.

h. Total buildout is the "total existing" development in the Specific Plan Area plus the "total net increase" of the proposed Project. The total buildout numbers are rounded up to the nearest tenth.



Source: PlaceWorks, 2015.

- Specific Plan Area Boundary
- Millbrae BART/Caltrain Station & Parking Structure
- Railroad
- TOD #1 Boundary
- TOD #2 Boundary

Figure 3-2
Proposed Project Boundary Map

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3.2 DESCRIPTION OF THE PROPOSED SPECIFIC PLAN UPDATE

3.2.1 EXISTING CONDITIONS

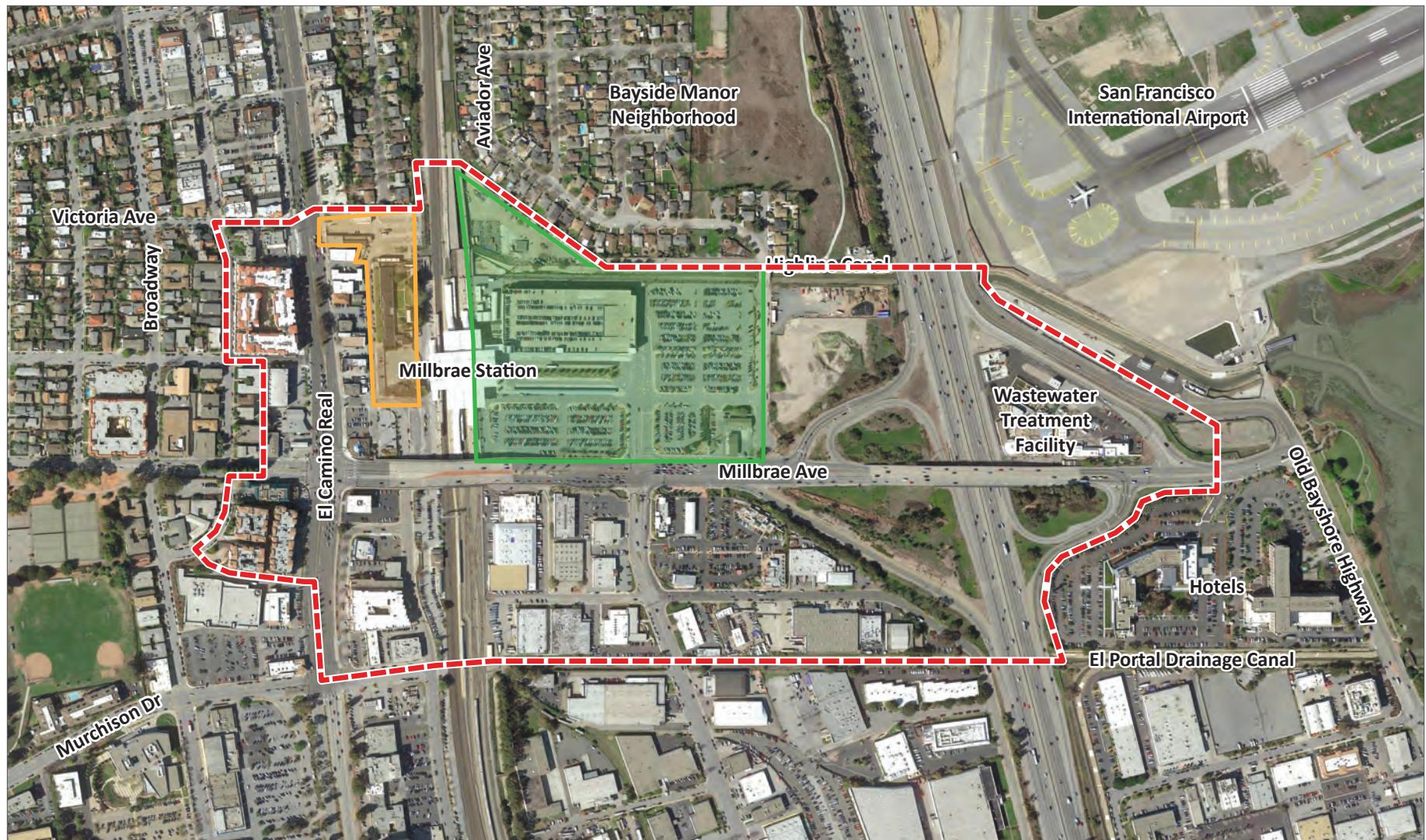
3.2.1.1 SPECIFIC PLAN AREA LOCATION

As shown in Figure 3-3, the Specific Plan Area is in a highly urbanized area. It is generally bound by El Camino Real and Broadway to the west; Victoria Avenue, single-family homes in the Bayside Manor neighborhood fronting Aviador Avenue, and the Highline Canal to the north; SFO, Old Bayshore Highway and two six-to seven-story hotels to the east; and the El Portal Canal, which makes up the common border with the City of Burlingame to the south.

3.2.1.2 EXISTING USES

The 116-acre Specific Plan Area is in one of the oldest areas of the city. As shown on Figure 3-3, the Specific Plan Area is currently occupied by various land uses, including commercial, light industrial, and multi-family residential. The following describes the land uses in the Specific Plan Area:

- **West of El Camino Real.** The west side of this six-lane corridor includes two mixed-use developments ranging from one- to five stories, containing residential and ground floor retail uses, as well as several residential service uses and restaurants.
- **East of El Camino Real – West of the Railroad Tracks.** The east side of this six-lane corridor, north of Millbrae Avenue, is mainly light industrial, including a lumberyard and auto repair shops, with a few food establishments. Uses between El Camino Real and the railroad tracks include convalescent homes and transportation-related uses, such as a taxi waiting area and a public parking lot. Uses in this area south of Millbrae Avenue include service commercial, including a bank, a motel, and restaurants.
- **East of Railroad Tracks – West of Highway 101.** The area north of Millbrae Avenue mainly consists of the Millbrae Station parking garage and surface parking lots. The City's storage yard is located north of the parking garage. The area between the parking lots, Highway 101 and associated on/off ramps, owned by SFO, is currently a vacant, highly disturbed, lot that is commonly used as a construction staging area by SFO. The area south of Millbrae Avenue includes light industrial uses, including storage structures, as well as some service and office uses including a car rental service, which occupy the majority of this area. One exception is a new retail development, which was recently completed on a site next to the Highway 101 on-ramp, on the east side of Rollins Road. This development includes a variety of retail and service uses, such as restaurants, a car wash, and a gas station.
- **East of Highway 101.** The area north of Millbrae Avenue includes the Water Pollution Control Plant (WPCP) and Wastewater Treatment Plant (WTP). The area to the south of Millbrae Avenue includes the Highway 101 on/off ramps surrounding the associated undeveloped, stormwater drainage area.



Source: Google Earth Pro, 2015.

0 500
Scale (Feet)

- [Red dashed box] Specific Plan Area Boundary
- [Orange box] TOD #1 Boundary
- [Green box] TOD #2 Boundary

Figure 3-3
Aerial Photograph of Specific Plan Location

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3.2.1.3 GENERAL PLAN AND ZONING

As shown on Figures 3-4 and 3-5, the Specific Plan Area has a General Plan land use designation of Millbrae Station Area Specific Plan (MSASP),⁴ and a Zoning designation of Millbrae Station Area Planned Development (MSAPD),⁵ respectively.

3.2.1.4 SAN FRANCISCO INTERNATIONAL AIRPORT LAND USE COMPATIBILITY PLAN

The *Comprehensive Airport Land Use Compatibility Plan for the Environs of San Francisco International Airport* is the Airport Land Use Compatibility Plan (ALUCP) for SFO. As required by State law, the SFO ALUCP is required to be updated every five years. The C/CAG, acting in its capacity as the Airport Land Use Commission for San Mateo County, adopted the current SFO ALUCP in November 2012. California law requires that, after an airport land use commission has adopted its ALUCP, affected local governments must update their general plans, specific plans, and land use regulations to be consistent with the ALUCP.⁶ As discussed in the SFO ALUCP, the SFO ALUCP recognizes that modifications to the Millbrae Station area is expected to improve service and ridership and subsequent need for TOD near the Millbrae Station, and that TOD also helps meet other regional and smart growth goals such as the reduction of vehicle miles traveled and improved air quality.⁷

Given the immediate proximity to SFO, the building regulations within the Specific Plan Update are highly sensitive to the policies and standards established by the SFO ALUCP.

The SFO ALUCP establishes planning boundaries around SFO that “define height/airspace protection, noise, and safety areas for policy implementation, and areas within which notification of SFO proximity is required as part of real estate transactions.”⁸ The SFO ALUCP has been prepared to be consistent with the guidance provided by the Department of Transportation, Division of Aeronautics, and the Federal Aviation Administration (FAA).

The SFO ALUCP presents criteria, maps, and policies to be utilized by the C/CAG Board of Directors (which serves as the SFO Airport Land Use Commission) and other local jurisdictions, including the City, when reviewing proposals for land use development within the Airport Influence Area (AIA), for the proposal’s compatibility with airport operations.

The AIA for SFO is composed of two areas: Area A and Area B. Area A is the larger of the two areas, and it encompasses all of San Mateo County. The Specific Plan Area is within both the boundary for AIA A and B.⁹

⁴ Millbrae 1998-2015 General Plan, Map 3-4, Land Use Map. Adopted November 24, 1998.

⁵ Millbrae Municipal Code, Title 10, Planning and Zoning, Chapter 10.05 Zoning, Article XVII, Millbrae Station Area Planned Development or “MSAPD” District.

⁶ California Government Code, Section 65302.3.

⁷ City/County Association of Governments of San Mateo County, 2012. *Comprehensive Airport Land Use Compatibility Plan for the Environs of San Francisco International Airport*, pp. II-11 to II-12.

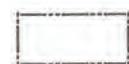
⁸ City/County Association of Governments of San Mateo County, 2012. *Comprehensive Airport Land Use Compatibility Plan for the Environs of San Francisco International Airport*, p.12.

⁹ *Comprehensive Airport Land Use Compatibility Plan for the Environs of San Francisco International Airport*, July 2012, Exhibit IV-1, page IV-3.

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Source: City of Millbrae General Plan.



City Boundary



TOD #1 Boundary



TOD #2 Boundary

Figure 3-4

Millbrae General Plan Land Use Map



Source: City of Millbrae, Official Zoning Map, October 13, 2009.

Figure 3-5
Millbrae Zoning Map

Generally, Area A contains areas overflowed by aircraft flying to and from SFO at least once per week at altitudes of 10,000 feet or less above mean sea level (MSL). Area B lies within Area A and contains areas exposed to aircraft noise equivalent level (CNEL) 65 decibel (dB) contour or lying below critical airspace.¹⁰

Development within the Specific Plan Area is subject to height limits imposed by the FAA for runways at SFO and the SFO ALCUP. Should the height limits established in this Specific Plan Update be higher than the SFO Critical Airspace Surfaces Map, the SFO ALCUP standards would supersede the Specific Plan Update. All development projects under the Specific Plan Update are required to be consistent with the SFO ALCUP, unless granted an exception by FAA, SFO, and other responsible agencies.

3.2.1.5 PRIORITY DEVELOPMENT AREA

The Metropolitan Transportation Commission (MTC) and Association of Bay Area Governments (ABAG) *Plan Bay Area* is the Bay Area's Regional Transportation Plan/Sustainable Community Strategy (SCS).¹¹ The Final *Plan Bay Area* was adopted on July 18, 2013.¹² The SCS sets a development pattern for the region, which, when integrated with the transportation network and other transportation measures and policies, would reduce greenhouse gas (GHG) emissions from transportation (excluding goods movement) beyond the per capita reduction targets identified by California Air Resources Board (CARB). Implementation of the *Plan Bay Area* would meet a 16 percent per capita reduction of GHG emissions by 2035 and a 10 percent per capita reduction by 2020 from 2005 conditions.¹³

In 2008, MTC and ABAG initiated a regional effort (FOCUS) to link local planned development with regional land use and transportation planning objectives. Through this initiative, local governments identified Priority Development Areas (PDAs). The PDAs form the implementing framework for *Plan Bay Area*. The PDAs are transit-oriented infill development opportunity areas within existing communities that are expected to host the majority of future development. Overall, well over two-thirds of all regional growth by 2040 is allocated within PDAs. The PDAs throughout the San Francisco Bay Area are expected to accommodate 80 percent (or over 525,570 units) of new housing and 66 percent (or 744,230) of new jobs.¹⁴

As shown on Figure 3-6, there are two PDAs within Millbrae: 1) El Camino Corridor, which encompasses a quarter-mile buffer along El Camino Real and 2) Transit Station Area, which is the Specific Plan Area. A segment of the El Camino Real Corridor PDA overlaps with the Transit Station Area PDA.

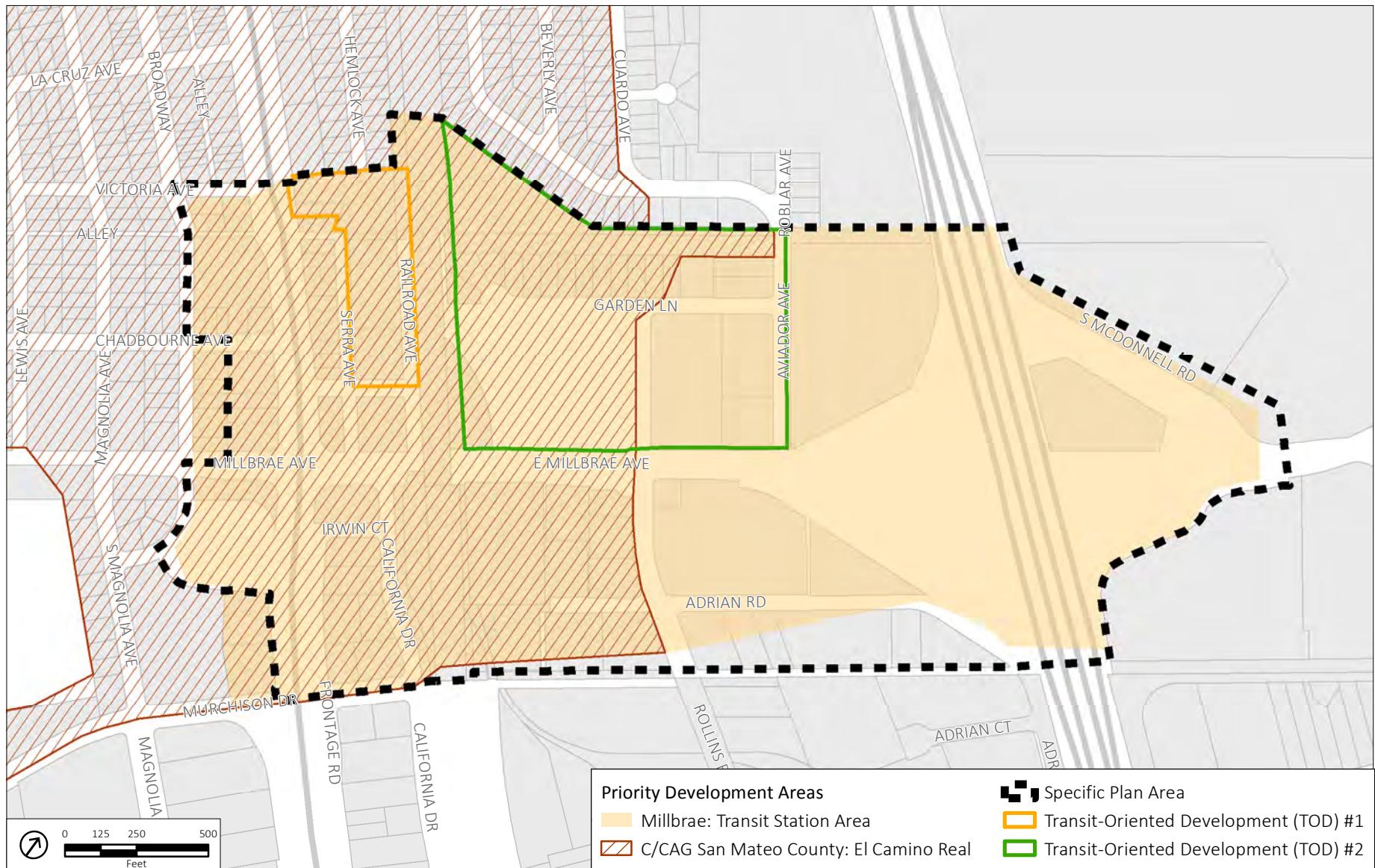
¹⁰ City/County Association of Governments of San Mateo County, 2012. Comprehensive Airport Land Use Compatibility Plan for the Environs of San Francisco International Airport, p. IV-2.

¹¹ To read more about *Plan Bay Area*: Jobs-Housing Connection Scenario, go to www.OneBayArea.Org.

¹² It should be noted that the Bay Area Citizens filed a lawsuit on Metropolitan Transportation Commission's and Association of Bay Area Government's adoption of *Plan Bay Area*.

¹³ Metropolitan Transportation Commission and Association of Bay Area Governments, 2013, *Final Plan Bay Area, Strategy for a Sustainable Region*, page 96.

¹⁴ Metropolitan Transportation Commission and Association of Bay Area Governments, 2013, *Final Plan Bay Area, Strategy for a Sustainable Region*.



Source: ABAG, 2014; PlaceWorks, 2014.

Figure 3-6
Millbrae Priority Development Areas

3.2.1.6 GRAND BOULEVARD INITIATIVE

El Camino Real, which is the subject of the Grand Boulevard Initiative, traverses the west side of the Specific Plan Area. The Grand Boulevard Initiative is a regional planning project intended to guide the revitalization of El Camino Real from San Jose to Daly City. This Initiative is a collaboration of 19 cities, counties, local, and regional agencies. The overall goal of the Initiative is to produce a coordinated series of policy decisions that will be embraced by all jurisdictions, thereby helping El Camino Real to function and look better.¹⁵

3.2.1.7 SURROUNDING USES

As shown on Figures 3-4 and 3-5 above, the Specific Plan Area is surrounded by various land use designations and zoning districts as follows:

- **North:** General Commercial, Parks and Open Space, and Low Density Residential land use designations and Downtown Improvement Area, Commercial, and Single Family zoning districts.
- **East:** Industrial and Utility, and General Commercial land use designations and Industrial and Planned Development zoning districts
- **South:** City of Burlingame to the south
- **West:** General Commercial and Medium Density Residential land use designations and Commercial and Multi-Family zoning districts

3.2.2 CONTENTS OF THE PROPOSED SPECIFIC PLAN UPDATE

The proposed Specific Plan Update contains the following chapters:

- **Chapter 1, Introduction:** This chapter describes the purpose and intent of the Specific Plan Update, the regional and local setting, the community planning process and the context with regards to other planning documents (e.g. Millbrae General Plan, Zoning, etc.) for the area.
- **Chapter 2, Summary of Existing Conditions:** This chapter provides an overview of existing conditions in the Specific Plan Area.
- **Chapter 3, Vision and Goals:** This chapter provides a vision statement and goals, which build upon the vision.
- **Chapter 4, Concepts and Policies:** This chapter describes the overall vision concepts and policies for land use, urban form, circulation, parking, housing, and utility infrastructure, which apply to the Specific Plan Area as a whole. (see Appendix J, Specific Plan Update Policies of this Draft EIR)
- **Chapter 5, Land Use Regulations and Development Standards:** This chapter stipulates permitted uses in the Specific Plan Area and provides specific building and site design standards for new development in the Specific Plan Area.

¹⁵ Grand Boulevard Initiative, www.grandboulevard.net, accessed November 10, 2014.

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- **Chapter 6, Design Standards and Guidelines:** This chapter provides design guidance for private development in the Specific Plan Area. Design standards and guidelines prescribe various successful approaches to building and site design.
- **Chapter 7, Circulation and Parking:** This chapter provides proposed vehicular, transit, pedestrian, and bicycle improvements and recommended parking strategies for the Specific Plan Area and the area immediately surrounding the Millbrae Station. The chapter also provides streetscape standards for several major corridors.
- **Chapter 8, Utilities and Public Services:** This chapter describes the potential impacts of development on utility infrastructure and public services, and improvements to accommodate future growth.
- **Chapter 9, Implementation and Financing Plan:** This chapter provides a strategy for administrating and implementing the Specific Plan over the next 25 years, focusing on the costs of new improvements identified in the Specific Plan Update and the potential funding sources available.

3.2.3 COMPONENTS OF THE SPECIFIC PLAN UPDATE

3.2.3.1 LAND USE CONCEPT

The Specific Plan Update proposes to transform the Specific Plan Area from its current commercial and light industrial character into a vibrant mixed-use district that builds on the significant activity generated by the regional intermodal Millbrae Station. Figure 3-7 illustrates the land use concept for the Specific Plan Area and includes the following planning zones:

- **Transit-Oriented Development.** The Specific Plan Update envisions a wide variety of uses in areas closest to the Millbrae BART/Caltrain Station (Millbrae Station), including the current BART parking lots, that take advantage of station proximity. Employment uses, retail, service, hotels, and multi-family housing uses will maximize transit trips. The Specific Plan Update promotes the integration of these uses on individual sites and within single projects. All of these uses will capitalize on transit, and the integration of residential and employment uses will ensure that there is activity in the station area during the day and in the evenings.
- **Residential Mixed Use.** The El Camino Real corridor is envisioned as a mixed-use corridor that primarily consists of multi-family residential development above ground floor retail and service uses within individual buildings, continuing the pattern of recent development along the corridor. The El Camino Real corridor will also function as a bridge between the existing residential uses west of the Specific Plan Area and new intensive uses in the TOD area. Additional residential uses on El Camino Real will complement Downtown businesses without adding extensive vehicle trips and will provide additional life to central Millbrae.
- **Retail Center.** In the southeast quadrant of the Specific Plan Area, the existing retail center (Wilson Plaza) will retain its commercial character and extend farther to the east to provide additional retail shopping, services, and dining opportunities that are appropriate for this freeway-proximate area.
- **Employment Center.** The area south of Adrian Road will accommodate office uses in new Class A buildings that can take advantage of freeway proximity and transit service. New housing development in the nearby TOD area will create opportunities for employees to live close to their workplaces, which is attractive to employers and creates further opportunities for walking and bicycling to work.

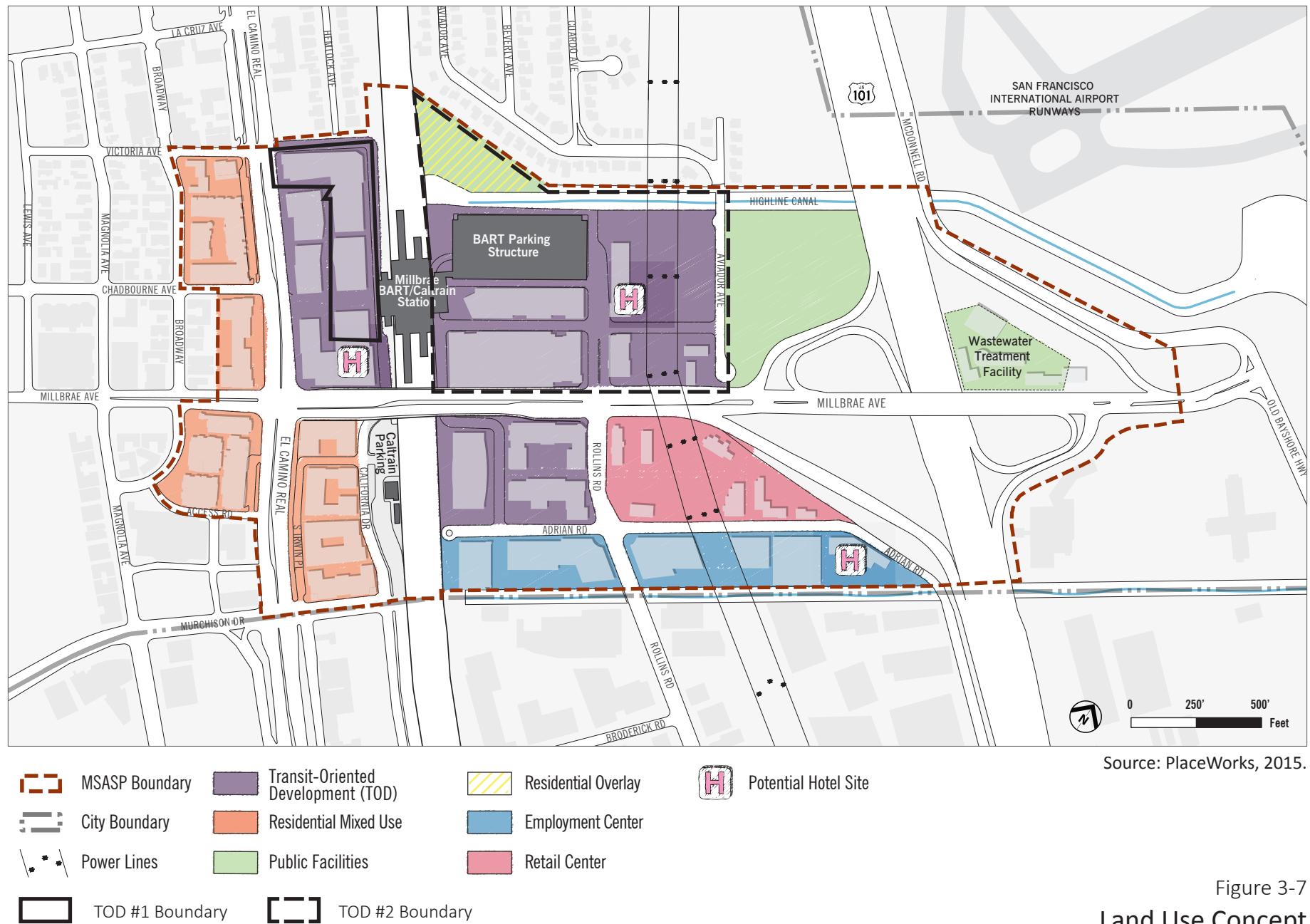


Figure 3-7
Land Use Concept

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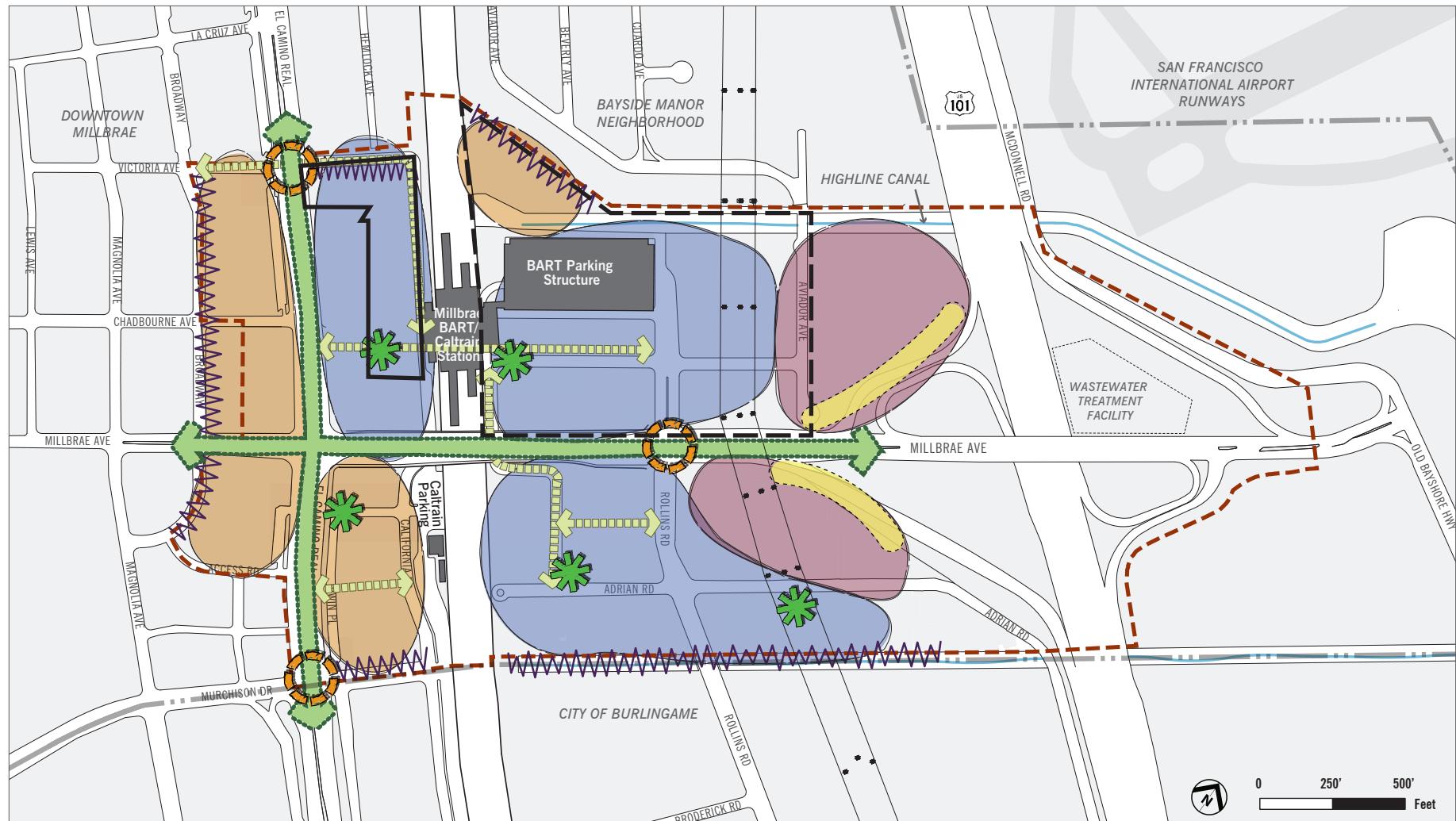
- **Hotels.** Hotels are envisioned in three locations that take advantage of freeway frontage, airport proximity, and transit access. Hotels will benefit both visitors and local residents and complement nearby retail and office uses. Hotels provide a meeting places for special events, conferences, or banquets, and diversify activities in the area, providing activity during the daytime and nighttime hours with lesser peak-hour traffic impacts. Hotels should be allowed flexibly and may be appropriate in TOD, Residential Mixed Use, Retail Center, and Employment Center areas.
- **Public Facilities.** Public Facilities are proposed to be used as public facilities. The area directly west of Highway 101 and north of Millbrae is restricted for development due to airport runway safety issues. As the Specific Plan Update is implemented, the area should be landscaped at its edges to provide for an attractive entry to the City from Highway 101. Potential uses for the area include stormwater treatment facilities (bioretention swales). The Wastewater Treatment Facility, shown in green, is proposed to continue its use as a public facility. It should be noted that additional, privately-owned public open spaces are envisioned for the Specific Plan Area and are discussed in greater detail below.
- **Multi-Family Residential Overlay.** The triangular-shaped area just north of the BART parking garage and south of the Bayside Manor neighborhood is envisioned for land uses that would provide a suitable land use transition between the BART station and the Bayside Manor neighborhood, including a city storage yard, parking, and multi-family residential uses. The Overlay Zone land use regulations shall be applied in addition to those in the underlying base zone.

3.2.3.2 URBAN DESIGN CONCEPT

The Specific Plan Update includes seven urban design principles that should be considered when evaluating future development projects and public improvements in the Specific Plan Area. Figure 3-8 illustrates many of these broad principles. The urban design principles focus on the physical form of future development and how that form can contribute to a pedestrian-friendly, active TOD for Millbrae that includes well-designed buildings and public spaces that are interconnected appropriately with key destinations and the surrounding urban context.

The urban design principles include the following:

- Provide Appropriate-Scale Development
- Facilitate Connections
- Create Public Open Spaces
- Activate Streets and Open Spaces
- Retain and Enhance Gateway Streets
- Take Advantage of Highway Frontage
- Transition to Surrounding Areas



Source: PlaceWorks, 2015.

Figure 3-8

Urban Design Concept

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3.2.3.3 DEVELOPMENT STANDARDS

The Specific Plan Update includes land use regulations and development standards to ensure that all proposed development in the Specific Plan Area supports the vision, goals, and concepts of the Specific Plan Update. The following provisions apply to all new development within the Specific Plan Area. If provisions in the Specific Plan Update and Chapter 10.5, Zoning, of the Millbrae Municipal Code are in conflict, the provisions in the Specific Plan Update shall be applied. For any zoning provisions not specifically addressed by the Specific Plan Update, the City's Zoning Code shall be applied.

Land Use Regulations

Figure 3-9 shows the Planning and Overlay Zones for the Specific Plan Update. Table 3-2 indicates the land uses that are permitted, conditionally permitted, and prohibited within each Planning or Overlay Zone. As described above, Overlay Zone land use regulations shall be applied in addition to those in the underlying base zone. Other uses not identified in Table 3-2 that are deemed consistent with the spirit and intent of the underlying land use designation shall be given consideration by the Community Development Director and approved at discretion of the Community Development Director or deferred to the Planning Commission.

TABLE 3-2 SPECIFIC PLAN UPDATE PERMITTED AND CONDITIONALLY PERMITTED LAND USES

Land Use Type	TOD	Residential Mixed Use	Office	Residential Overlay	Retail Commercial	Public Facilities ^a
Residential Uses						
Duplexes	-	-	-	C	-	-
Live/Work Units	P*/C* ^b	P*	C	-	-	-
Multiple-Family Dwellings						
Less than 7 Units	-	-	-	C	-	-
7 Units or More	P/C ^b	P	C	C	-	-
Public and Quasi-Public Uses						
Adult Education	P*	P*	P*	-	P	-
Community Centers ^c	P	P	P	-	P	-
Public Electric Vehicle Charging Stations	P*	P*	P*	-	P	-
Library	P	P	P	-	P	-
Public Parks and Recreational Facilities ^c	P	P	P	P	P	P
Commercial Uses						

PROJECT DESCRIPTION

TABLE 3-2 SPECIFIC PLAN UPDATE PERMITTED AND CONDITIONALLY PERMITTED LAND USES

Land Use Type	TOD	Residential Mixed Use	Office	Residential Overlay	Retail Commercial	Public Facilities ^a
<i>Eating and Drinking Establishments</i>						
Restaurants	P*	P*	P*	-	P	-
Bars	C*	C*	C*	-	C	-
<i>Commercial Services</i>						
Banks and Financial Services	P*	P*	P*	-	P*	-
Business Support Services	P*	P*	P*	-	C*	-
Child Care Services ^d	C*	C*	C*	-	-	-
Health and Exercise Clubs (no more than 3,000 square feet if on ground floor)	P*	P*	P*	-	P*	-
Medical Offices	P*	C*	C	-	-	-
Professional Offices	P	P*	P	-	-	-
Personal Services	P*	P*	P*	-	P*	-
<i>Retail</i>						
Drive-In and Drive-Through Facilities	-	-	-	-	P	-
Food and Beverage Sales (less than 15,000 square feet)	P*	P*	P*	-	P	-
Gas and Service Stations	-	-	-	-	C	-
Retail Sales	P*	P*	P*	-	P	-
Liquor Stores	C*	C*	-	-	C*	-
<i>Other Commercial Uses</i>						
Commercial Lodging	P	P	P	-	P	-
Conference Center	P*	C	P*	-	P*	-
Indoor Commercial Recreation	P*	-	-	-	P	-
Research & Development Facilities	C	-	P	-	-	-
<i>Transportation, Communication, and Utilities Uses</i>						
Off-Site Construction Staging	C	C	C	-	P	-

PROJECT DESCRIPTION

TABLE 3-2 SPECIFIC PLAN UPDATE PERMITTED AND CONDITIONALLY PERMITTED LAND USES

Land Use Type	TOD	Residential Mixed Use	Office	Residential Overlay	Retail Commercial	Public Facilities ^a
Wireless Communications Facilities	C	C	C	-	C	C
Transit Facilities	P	-	-	-	-	-
Utility Services	C	-	C	-	C	P

Notes: Permitted (P), Conditional (C), Not allowed (-), Permitted when part of mixed use building (*)

Any use that requires a Conditional Use Permit (C) goes to the Planning Commission for approval.

Within SFO Safety Compatibility Zones 1, 2, and 3, uses are subject to Table IV-2, Safety Compatibility Criteria of the San Francisco Airport Land Use Compatibility Plan, adopted in November 2012, or the latest adopted plan. Subject to City/County Association of Governments(C/CAG)'s review for land use compatibility.

a. Within SFO Safety Compatibility Zone 1, no new structures are allowed except for structures serving specific aeronautical functions in compliance with applicable FAA design standards.

b. Residential uses on the east side of the railroad tracks require a Conditional Use Permit (C).

c. Places of assembly seating more than 300 people are prohibited in SFO Safety Compatibility Zone 2. Places of assembly not in structure are prohibited in SFO Safety Compatibility Zone 1.

d. Large child day care centers, which are commercial facilities defined in accordance with Health and Safety Code, Section 1596.70, et seq., and licensed to serve 15 or more children, are prohibited in SFO Safety Compatibility Zones 2 and 3. Family day care homes and noncommercial employer-sponsored facilities ancillary to place of business are allowed in SFO Safety Compatibility Zone 3.

Specific Plan Policies

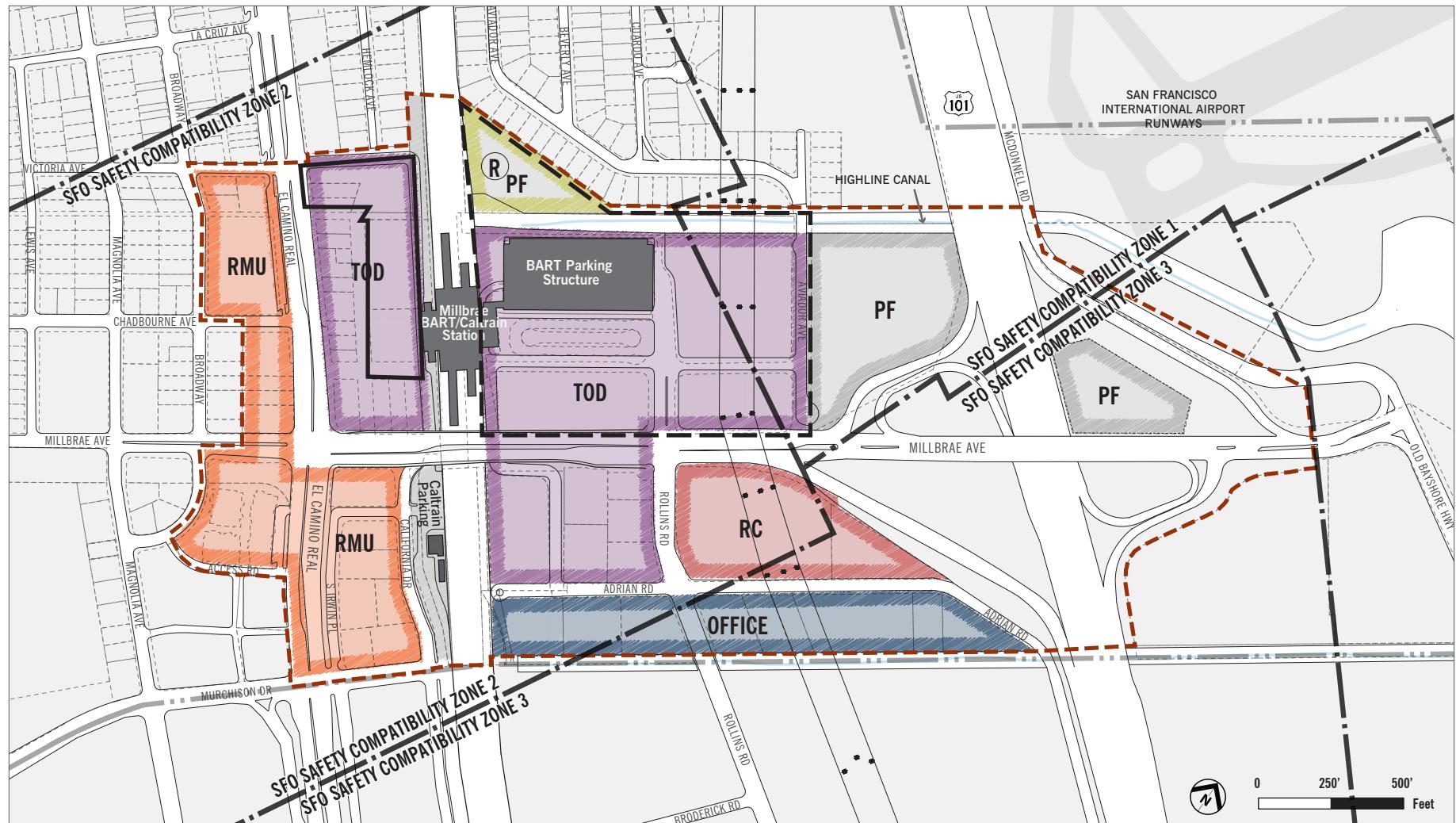
The Specific Plan Update includes policies to help achieve the vision and goals of the Specific Plan Update. A complete list of policies that shall be observed in all development in the Specific Plan Area is provided in Appendix J of this Draft EIR.

Development Standards

The Development Standards in this section regulate new site and building development through standards for intensity, building height, setbacks, open space, parking, and other elements. Development Standards in Table 3-3 are provided for the Planning and Overlay Zones described above.

Height Limits

Maximum and minimum building heights are established for projects in the Specific Plan Area to achieve a building scale that is transit-supportive, as well as pedestrian-friendly. Under the Specific Plan Update, the height of a building or structure is the measurement of the greatest vertical distance above the exterior finished grade to the highest point of the building immediately above, exclusive of antennas, chimneys and roof equipment. The height of a stepped or terraced building is the height of the tallest segment of the building. Figure 3-10 illustrates height limits in the Specific Plan Area. As previously stated in Section 3.2.1.4, San Francisco International Airport Land Use Compatibility Plan, heights must be consistent with the SFO ALUCP.



Source: PlaceWorks, 2015.

MSASP Boundary

City Boundary

Power Lines

TOD #1 Boundary

Planning Zones

- Transit-Oriented Development (TOD) Zone
- Residential Mixed Use (RMU) Zone
- Office Zone

- Retail Commercial (RC) Zone
- Public Facilities (PF) Zone

Overlay Zone

- Residential Overlay
- SFO Safety Compatibility Zones*

* Subject to C/CAG review
for land use compatibility

Figure 3-9

Planning and Overlay Zones

PROJECT DESCRIPTION

TABLE 3-3 DEVELOPMENT STANDARDS BY PLANNING AND OVERLAY ZONES

	TOD	Residential Mixed-Use	Office	Residential Overlay	Retail Commercial	Public Facilities
Height						
Height (Max. ^a /Min. ^b)	Min. 2 stories	Min. 2 stories	Min. 2 stories	-	-	-
Ground Floor Height (Min.) ^c	15 feet	15 feet	15 feet	-	15 feet	-
Maximum FAR and Residential Density^d						
Max. FAR (Non-Residential)	5.75	1.0 ^e	2.0	0	1.0	0.3
Max. Upper Floor Residential Density (du/ac)	250 ^f	80	60 ^f	30	0	0
Setbacks^g						
Front Setback						
Minimum Interior Setback (side, rear)	5 feet	5 feet	10 feet	10 feet	-	N/A
Open Space (Min.)^h						
Required Common Open Space (% of site area)	25%	10%	20%	20%	10%	N/A
% of Required Common Open Space Accessible to Public	50%	25%	25%	-	100%	N/A
Development Scale (Max.)						
Building Floor Plate ⁱ	45,000 sf	30,000 sf	30,000 sf	-	-	-
Affordable Housing for Very Low, Low, and Moderate Incomes (% of Units) ^j	15%	15%	15%	N/A	N/A	N/A
Utility Infrastructure ^k	See Section 3.2.3.5, Utilities, below.					

Notes: FAR = Floor Area Ratio, du/ac = dwelling units per acre, sf = square feet, Max. = maximum, Min. = minimum

a. Heights must be compatible with the San Francisco International Airport Land Use Compatibility Plan. Maximum height is shown on Figure 3-10.

b. Exception allowed for a signature building that is part of a larger development.

c. Measured floor plate to floor plate.

d. Floor Area Ratio (FAR): The ratio of the gross floor area of all buildings on a lot to the area of the lot.

e. If office development, maximum FAR = 1.5

f. Dwelling units count towards FAR

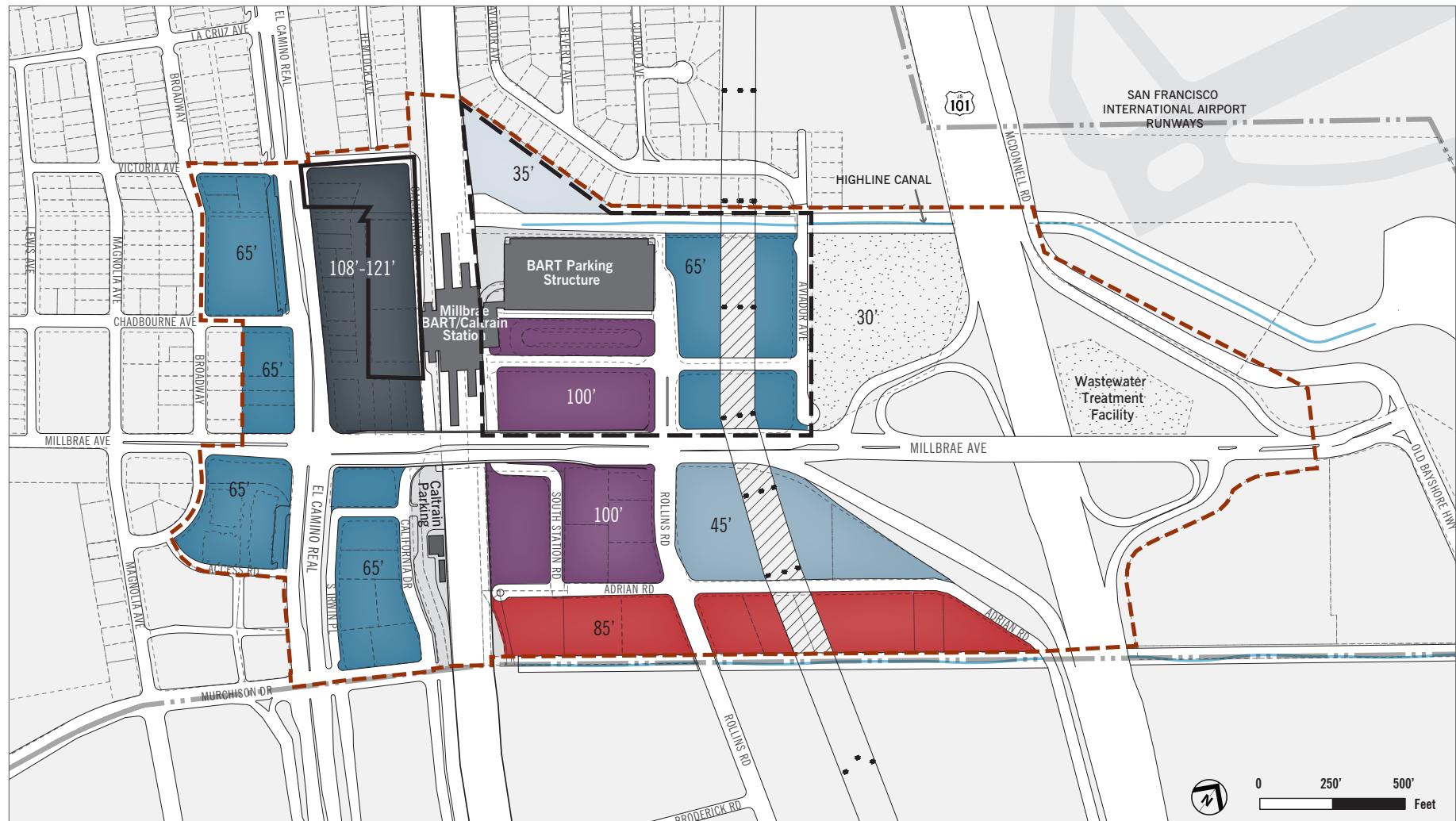
g. Setbacks are shown on Figure 3-11 below.

h. Subject to provisions of Chapter 10.20, Park and Recreation Land, of Millbrae Municipal Code.

i. For mixed-use projects, a parking garage's floor-plate does not count towards this requirement, provided that the parking garage is concealed by active uses, such as retail stores, and is no more than one-story high. In such case, the floor-plate size of individual residential or office buildings on top of the parking garage shall not exceed the maximum floor-plate identified in this table. Individual infill projects on parcels with frontage of less than 100 feet are exempt from this requirement.

j. Affordable housing requirement should be compatible with applicable law and implemented through an agreement negotiated with the City.

k. All development projects shall install recycled water facilities for irrigation and provide connections to the City's fiber optic network.



Source: PlaceWorks, 2015.

- MSASP Boundary
 - City Boundary
 - Power Lines
 - TOD #1 Boundary
 - TOD #2 Boundary
- | | |
|-----------|-----|
| 108'-121' | 65' |
| 100' | 45' |
| 85' | 35' |
- Maximum Height*
- * Heights must be compatible with the San Francisco International Airport Land Use Compatibility Plan

Figure 3-10
Height Limits

PROJECT DESCRIPTION

Setbacks

The Specific Plan Update establishes standards for building edges adjacent to specific streets in the Specific Plan Area. The Specific Plan Update also establishes building setback lines based on property lines. If the existing sidewalk width equals or exceeds the minimum shown in Table 3-4, the setback line equals the property line. If the existing sidewalk is less than the minimum width shown in Table 3-4, the setback line is set back from the property line the distance needed to achieve the minimum sidewalk width. Minimum interior and side setbacks are shown in Table 3-3 above; however, other setback standards are determined by street segment because of varying conditions, including widths, character, and use of the street. These are shown in Table 3-4 and illustrated in Figure 3-11.

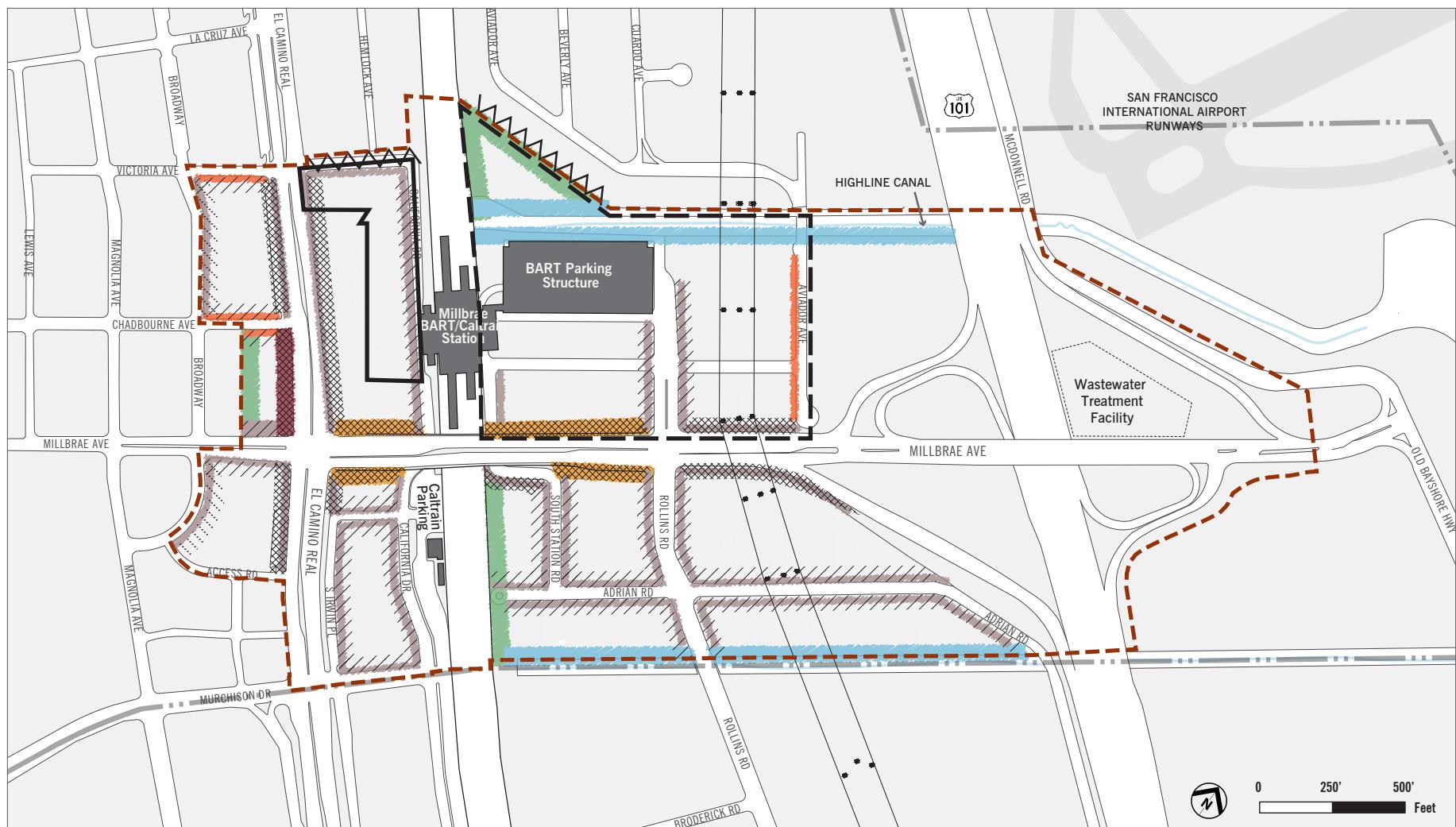
TABLE 3-4 **STREET-BASED BUILDING FRONTAGE STANDARDS**

Street	Building Setback (Minimum)	Sidewalk Width (Minimum)	Street Wall Height At Setback Line (Maximum)
El Camino Real	Built to Setback Line Exception: 42' setback required to provide frontage roads on the west side of El Camino Real	12 feet	65 feet
Millbrae Avenue	Built to Setback Line	12 feet	65 feet
Millbrae Overpass	20' from the edge of the overpass structure to provide emergency access [1]	10 feet	65 feet
Rollins Road, South Irwin Place, Adrian Road, South Station Road (New Street), California Drive, Murchison Drive	Built to Setback Line	12 feet	55 feet
Victoria Avenue, Chadbourne Avenue	Built to Setback Line	12 feet	55 feet
Broadway	Built to Setback Line	12 feet	35 feet
Highline Canal and El Portal Canal	35' from the top of the bank to provide emergency access	N/A	N/A
Next to Bayside Manor Neighborhood	20' from Property Line	N/A	20 feet
Areas/Streets not Identified in this Table	Built to Setback Line	8 feet	N/A

Source: Draft Millbrae Station Area Specific Plan Update, 2015.



PROJECT DESCRIPTION



Source: PlaceWorks, 2015.

Figure 3-11

Sidewalk and Setback Requirements

PROJECT DESCRIPTION

Setbacks Near Single-Family Designations

If a new building would be built across the street from areas designated as single-family residential, the building height shall not exceed the line drawn at a 45-degree angle starting at the edge of the residential building's parcel line. If a new building would be built adjacent to areas designated as single family residential, the building shall set back 20 feet from the property line of the adjacent residential building, and the building height shall not exceed the line drawn at a 45-degree angle starting at the edge of the residential building's parcel line.

Other Setback Standards

The Specific Plan Update requires two types of active frontage treatments as follows:

- **Active Frontage Type 1.** This frontage type is intended to provide active ground floor uses to attract pedestrian traffic and create a vibrant streetscapes and includes the following standards:
 - Active ground floor uses, including retail, restaurant, services, lobby entries, live/work, and community gathering spaces, are required to fill a minimum of 65 percent of the length of the building.
 - 75 percent of the linear ground floor wall shall be transparent (consisting of windows and doors).
- **Active Frontage Type 2.** This frontage type is intended to create street frontages that are visually attractive and provide a sense of place and security when active ground floor uses are not applicable.
 - Residential uses are allowed if residential entry doors are directly accessible from the sidewalk.
 - Office uses are allowed if a minimum of 40 percent of the linear ground floor wall is transparent, and interior uses are visible from the sidewalk.
 - Use landscaping and public art to screen blank walls.
 - Structured parking shall be screened from pedestrian views.

Additionally, the Specific Pan Update includes standards for maximum street wall heights to break up building mass, lower the perceived building heights, and minimize shadow impacts. Upper floors beyond the street wall height limits shall be set back a minimum of 10 feet. Exceptions may be granted for architectural interest at the Community Development Director's discretion.

Usable Open Space

The Specific Plan Update requires usable open space, which means outdoor or unenclosed areas on the ground, decks, porches, or terraces that are designed and accessible for outdoor living, recreation, pedestrian access, or gardening. Private open space, common open space, and publicly accessible open spaces are all counted in calculating usable open space. Streets, off-street parking and loading areas, driveways, utility or service areas, and required sidewalks are not counted as usable open space. Under the Specific Plan Update, usable open space shall provide trees, seating, landscaping, shading, and lighting. All development projects are required to provide publicly accessible open space in the forms of plazas, terraces, front courts, rooftop gardens, and pocket parks within the TOD, Residential Mixed Use, Office, and Retail Commercial zones, which shall be maintained by the developers.

Parking Standards

Vehicular Parking Standards

The minimum parking requirements outlined in Table 3-5 apply to developments that do not employ other parking management techniques. Developments that incorporate other parking and transportation demand management techniques to reduce demand, including shared parking, employer Transportation Demand Management (TDM) programs, subsidized transit passes, and rideshare incentives, could result in negotiations between the City and developers to provide lower parking requirements.

TABLE 3-5 MINIMUM OFF-STREET PARKING REQUIREMENTS

Land Use	Parking Ratios	
	Transit-Oriented (within 800 feet of station) 1.5 spaces per 1,000 gross square feet (gsf)	General 2.5 spaces per 1,000 gsf
Office	(within 800 feet of station) 0.4 space per room	1 space per room
Residential	(within 600 feet of station) 1 space per unit	1.25 spaces per unit
Restaurant	(within 600 feet of station) 5.0 spaces per 1,000 gsf	6.5 spaces per 1,000 gsf
Retail	(within 600 feet of station) 1.5 spaces per 1,000 gsf	3 spaces per 1,000 gsf

Notes: gross square feet = gsf

Source: Draft Millbrae Station Area Specific Plan Update, 2015.

Bicycle Parking Standards

Bicycle parking generally falls into two categories: long-term (Class I) and short-term (Class II). Long-term bicycle parking serves parking needs of longer than two hours and is used by bicyclists who prioritize greater security and protection from the elements. Short-term bicycle parking serves parking needs of shorter than two hours and is used by bicyclists who prioritize convenience and accessibility. Bicycle parking rates are shown in Table 3-6.

PROJECT DESCRIPTION

TABLE 3-6 BIKE PARKING REQUIREMENTS

Activity Type	Long-Term Bicycle Parking Requirements	Short-Term Bicycle Parking Requirements
Multi-Family Residential	0.5 spaces for each bedroom. Minimum requirement 2 spaces.	0.05 spaces for each bedroom. Minimum requirement 2 spaces.
Office	1 space per 10,000 square feet of floor area. Minimum requirement 2 spaces. OR Adequate spaces to accommodate 5% of building users (measured at peak periods).	1 space per 20,000 square feet of floor area. Minimum requirement 2 spaces.
Retail	1 space per 12,000 square feet of floor area. Minimum requirement 2 spaces.	1 space per 2,000-5,000 square feet of floor area. Minimum requirement 2 spaces.
Off-Street Parking Lots and Garages Available To General Public	1 space per 20 automobile spaces.	1 space per 20 automobile spaces. Minimum requirement 6 spaces.
Millbrae Station	Adequate spaces to meet existing demand plus an additional 10% for future growth.	Adequate spaces to meet existing demand plus an additional 10% for future growth.

Source: Association of Pedestrian and Bicycle Professionals, Bicycle Parking Guidelines, 2nd Edition, 2010; Bay Area Rapid Transit *Bicycle Access and Parking Plan*, 2002.

3.2.3.4 TRANSPORTATION AND CIRCULATION

Vehicular Street Circulation Improvements

The Specific Plan Update includes transportation improvements to accommodate all travel modes and an intensification of development in the vicinity of the Millbrae Station and will make travel by foot, bicycle, bus, and rail more efficient and attractive. The Specific Plan Update includes the following proposals that will improve vehicular access and circulation within the Specific Plan Area, shown in Figure 3-12:

- **Extend California Drive.** California Drive should be extended north from Linden Avenue to form the eastern leg of the signalized intersection at El Camino Real / Victoria Avenue. This roadway extension must be conducted in conjunction with development on the west side of the Millbrae Station and must permit bike and pedestrian use.
- **Rollins Road Reconfiguration.** North of Millbrae Avenue, Rollins Road should be reconfigured to accommodate vehicle traffic accessing development anticipated east of the station. This reconfiguration may include signalization of the internal intersection of Rollins Road / Garden Lane, if determined to be necessary.

- **Victoria Avenue Reconfiguration.** Convert Victoria Avenue from one-way traffic to two-way traffic between El Camino Real to Magnolia Avenue. With the addition of new land uses and traffic created in the Plan Area, Victoria Avenue will become an important access point, linking Downtown Millbrae and the Station. Converting Victoria Avenue will help alleviate congestion at the El Camino Real / Millbrae Avenue intersection. Two-way operation can be accommodated on Victoria Avenue within the existing right-of-way by removing the existing on-street parking and by updating the existing signal at its intersection with El Camino Real.
- **Create South Station Road.** In the short-term, the existing exit road from the east side of Millbrae Station shall remain. New development shall convert South Station Road from one-way to two-way traffic between Garden Lane and Adrian Road. This expansion will increase vehicle and bicycle connectivity between planned land uses south of Millbrae Avenue with the station, the rest of the Plan Area, and the city.

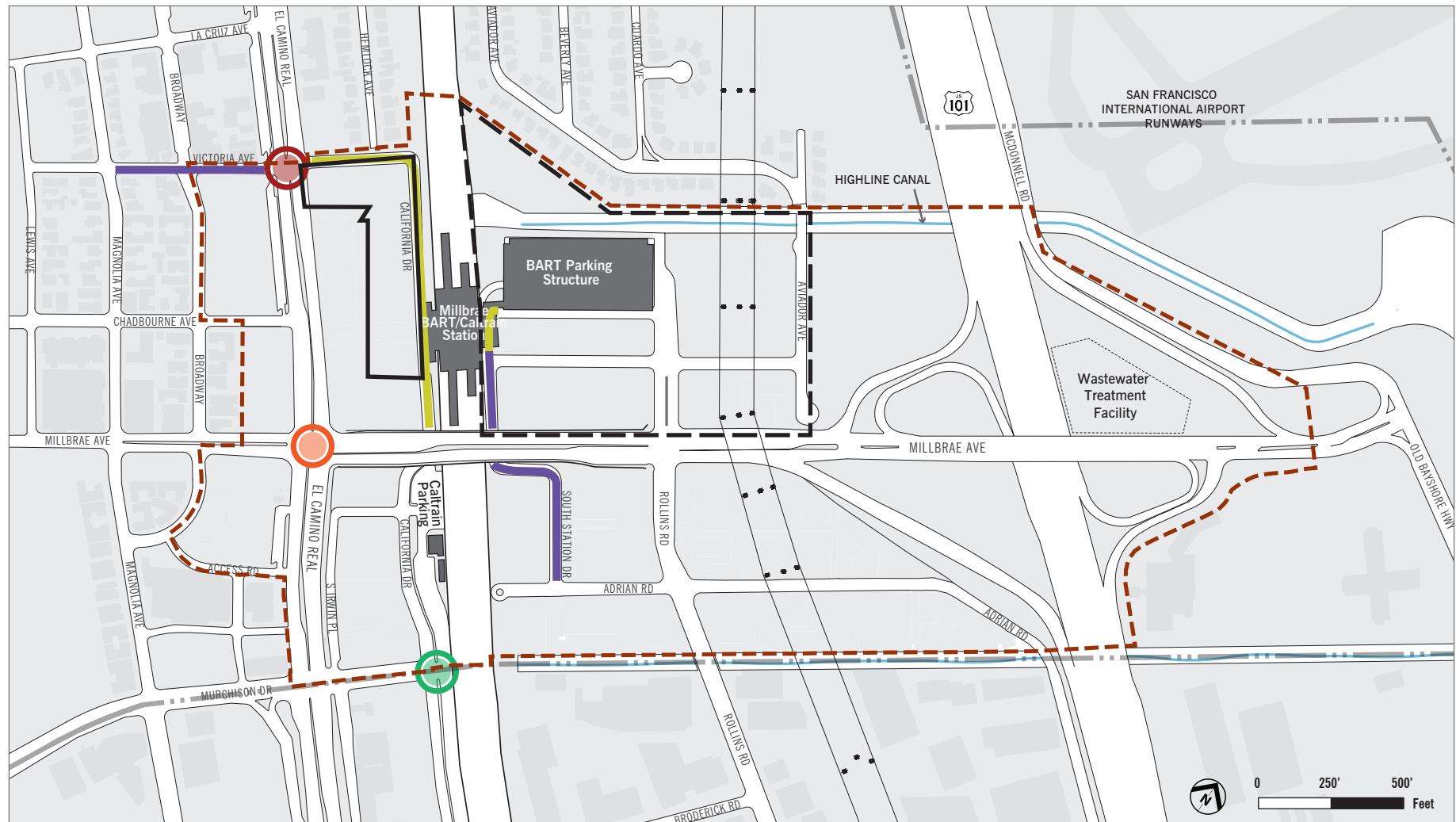
Bicycle Improvements

The proposed Specific Plan Update's bicycle circulation framework is shown on Figure 3-13. The Specific Plan Update proposes a variety of bicycle facility types, depending on the specific location. These include Class I Bikeways/Bicycle Paths, Class II Bicycle Lane, and Class III Bicycle Route. Bicycle access to Millbrae Station and bicycle connections among the surrounding land uses is provided through a system of on-street and off-street bicycle facilities. The Specific Plan Update increases separation from other road users with Class I bicycle paths and Class II bicycle lanes, and increases bicycle visibility to other road users through enhanced treatments at intersections, including bicycle signal detection and colored pavement markings. The proposed Specific Plan Update prioritizes bicycle circulation along smaller interior streets and includes a connection to the planned Bay Trail. As described above in the discussion on parking, the Specific Plan Update includes requirements for both long-term and short-term bicycle parking.

Pedestrian Improvements

The Specific Plan Update's pedestrian circulation framework is shown on Figure 3-14. The proposed Specific Plan Update prioritizes pedestrian circulation along all streets, with new connections through development projects and enhancements at major intersections. As stated above, a connection to the planned Bay Trail is also proposed. As shown in Figure 3-14, the Specific Plan Update includes pedestrian intersection enhancements at the following two uncontrolled intersections on Rollins Road near the Millbrae Station and the following controlled intersections:

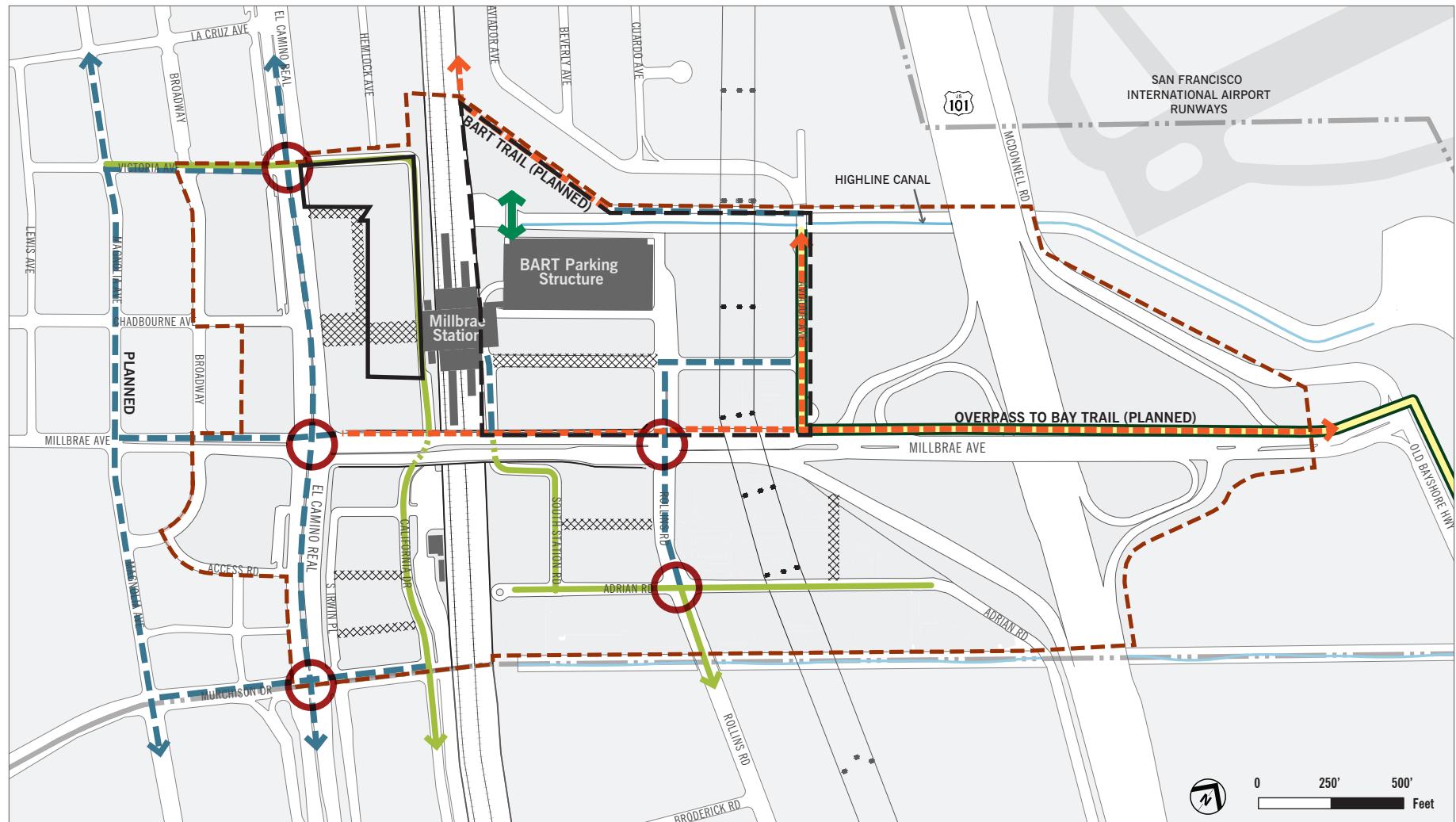
- El Camino Real/Victoria Avenue
- El Camino Real/Millbrae Avenue
- El Camino Real/Murchison Drive
- California Drive/Murchison Drive
- Rollins Road/Adrian Road
- Rollins Road/Millbrae Avenue



Source: PlaceWorks, 2015.

- MSASP Boundary
- City Boundary
- Power Lines
- TOD #1 Boundary
- TOD #2 Boundary
- Convert Road to Two-Way Traffic (Long Term)
- New Roadway Configuration
- New Signal
- Update Existing Timing & Coordination Along El Camino Real and Millbrae Ave
- Update Signal to Include New Eastern Leg

Figure 3-12
Vehicle Circulation Concept

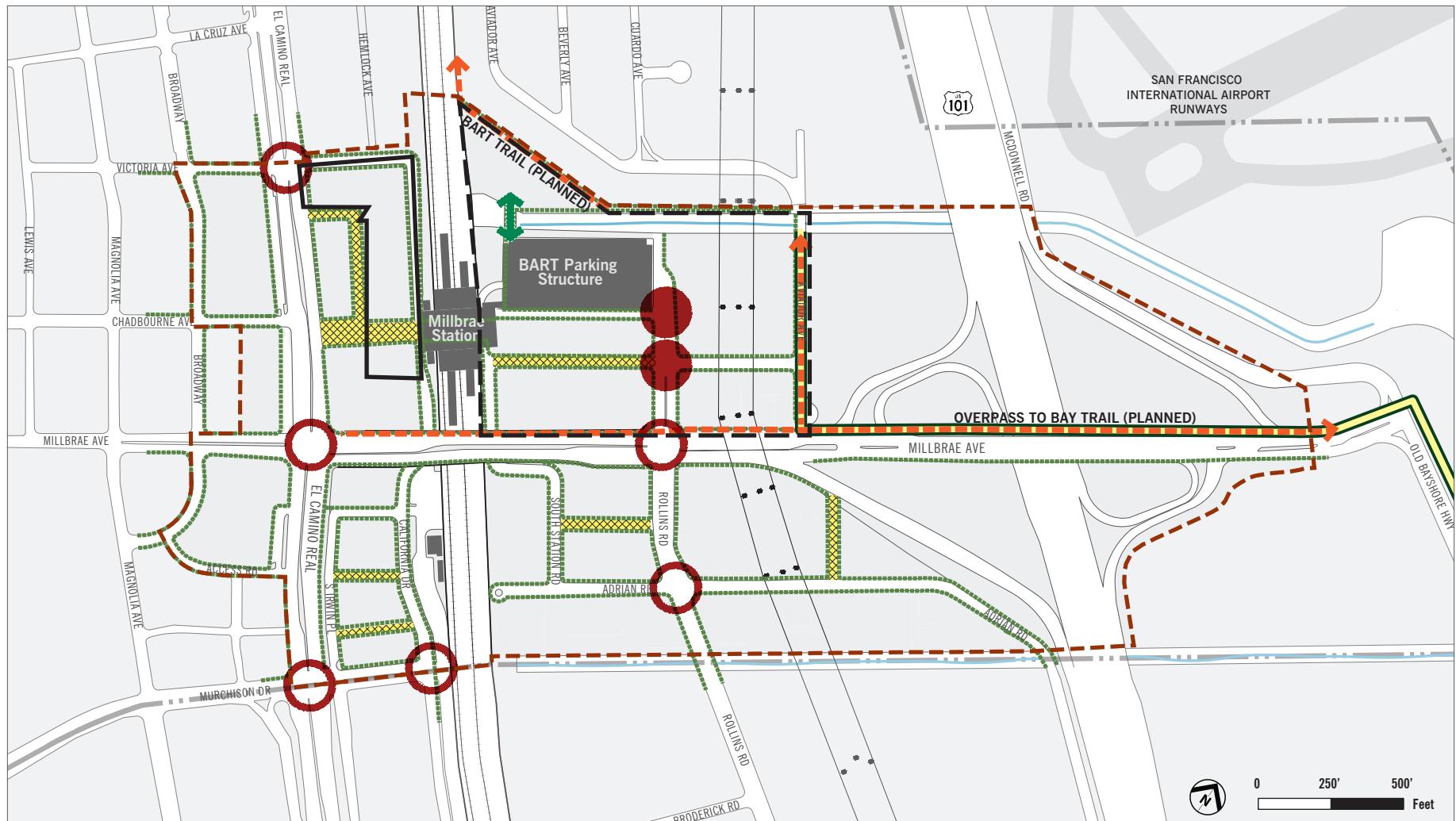


Source: PlaceWorks, 2015.

- MSASP Boundary
- City Boundary
- Power Lines
- TOD #1 Boundary
- TOD #2 Boundary
- Suggested Bike Facilities
 - Class I Multi-Use Path
 - Class II Bike Lane
 - Class III Bike Route
 - ↔ Potential Bike/Ped Bridge
 - Bay Trail
- Intersection Enhancement

Figure 3-13

Bicycle Circulation Concept



Source: PlaceWorks, 2015.

MSASP Boundary

City Boundary

Power Lines

TOD #1 Boundary

Suggested Pedestrian Facilities

Continuous Sidewalk Network

Multi-Use Path

Potential Pedestrian Paseo

TOD #2 Boundary

Bay Trail

Potential Bike/Ped Bridge

Improved Crossing (Controlled)

Improved Crossing (Uncontrolled)

Figure 3-14
Pedestrian Circulation Concept

Transit

The Specific Plan Area is centered on the Millbrae Station, which is the preeminent Bay Area intermodal station for BART (heavy rail) and Caltrain (commuter rail). In addition, rubber-tired transit bus and shuttle connections provide access to and from the Specific Plan Area. Because the Specific Plan Area will transform from primarily an origin station today (users arrive and go elsewhere) to both an origin and destination (users arrive to Millbrae from elsewhere) at buildout, the Specific Plan Update includes a focus on convenient, proximate and safe access to all the transit modes that serve it. Figure 3-15 shows a proposed transit network. The Specific Plan Update includes improvements to the Millbrae Station area wayfinding and directional signage. The Specific Plan Update anticipates eventual evolution of local bus service to Rapid or BRT-style service on El Camino Real by 2040 with exclusive travel lanes in certain segments, signal priority, enhanced and more robust stations, and other features to improve reliability, speed, in-vehicle experience, and passenger waiting experience. Transit stop enhancements, including benches, canopies, real-time passenger information displays, windscreens, lighting, signage and new platforms are included in the Specific Plan Update.

3.2.3.5 UTILITIES

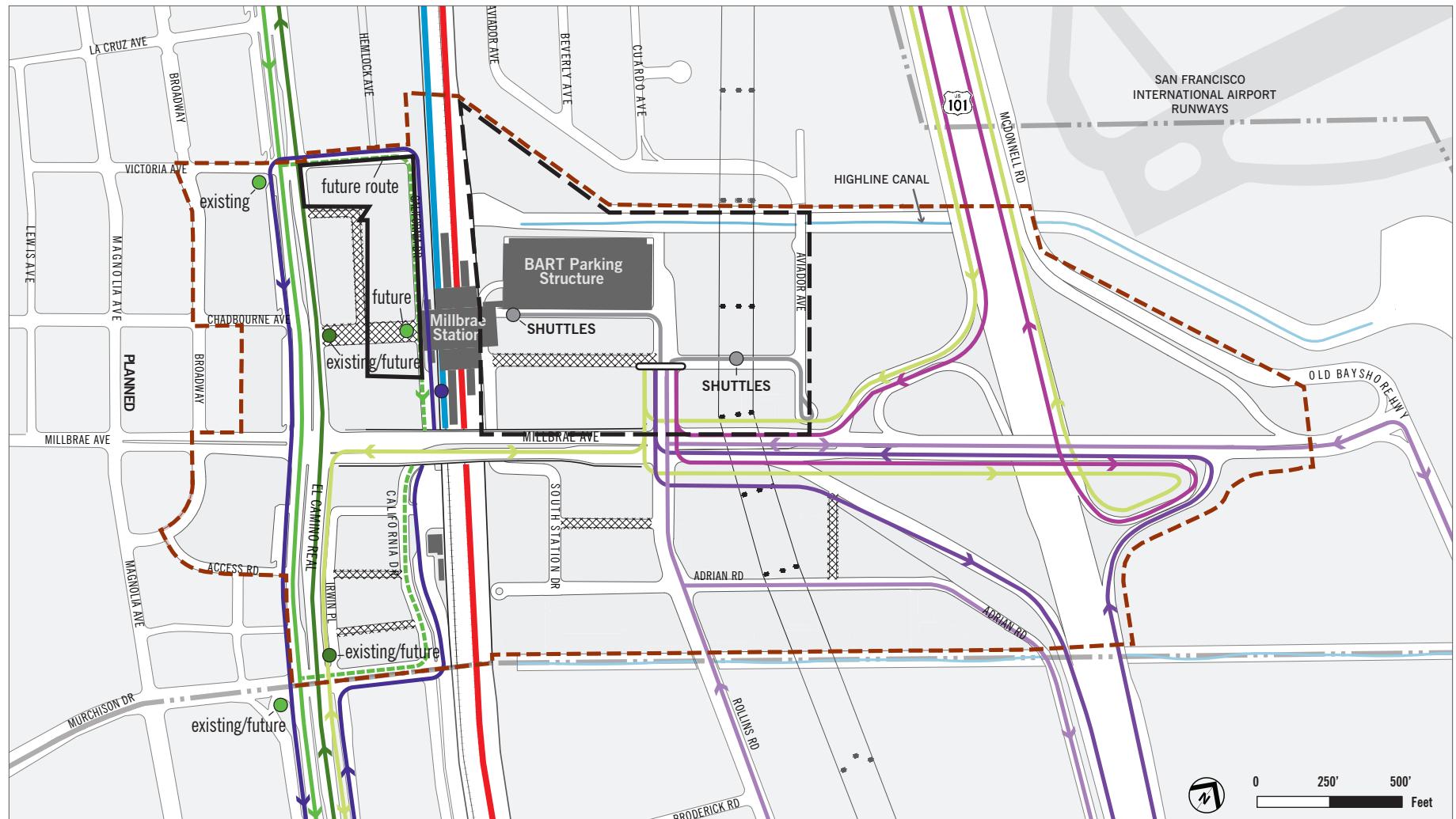
Infrastructure improvements will need to be phased with private development to ensure that sewer, water, drainage, fiber-optic cable, and energy services are provided, and that developers contribute their fair share to the costs of facility upgrades. The phasing of utilities will be critical to ensure that citywide systems retain sufficient capacity to meet demands. This following describes utility upgrades that would be needed to serve Specific Plan Area buildout.

Water Supply and Conservation

Millbrae obtains all of its water through a contract with the San Francisco Public Utilities Commission (SFPUC). This water is delivered from the City and County of San Francisco's Regional Water System (RWS), operated by the SFPUC. SFPUC's supply is predominantly from the Sierra Nevada, delivered from the Hetch Hetchy Reservoir through the Hetch Hetchy aqueducts, but also includes treated water produced by the SFPUC from its local watersheds and facilities in Alameda and San Mateo Counties. The SFPUC and the City (and the other wholesale customers) entered into a Water Supply Agreement in July 2009, which has a 25-year term. Millbrae's Individual Supply Guarantee (ISG) is 3.15 mgd.

The Millbrae distribution system includes 11 pressure zones, 6 pumps (3 each at 2 stations), 5 storage tanks (only 4 are in operation; 1 is standby), 568 hydrants, and 69.7 miles of water mains. Water mains in the Specific Plan Area range from 6 to 12 inches in diameter, with a mix of materials such as asbestos-cement, cast iron, and PVC pipe. The Specific Plan Area is within the city's Pressure Zone #4. Tank storage has not yet been developed in this pressure zone due to the direct connection to the Hetch Hetchy transmission infrastructure through this area.

Currently no recycled water facilities exist within the Specific Plan Area.



Source: PlaceWorks, 2015.

Figure 3-15

Transit Circulation Concept

PROJECT DESCRIPTION

Since several streets will be removed or modified as a result of the Specific Plan Area buildout, existing water lines will need to be relocated to adjoining street areas. A proposed schematic of the water line distribution system within the Specific Plan Area is shown in Figure 3-16.

The Specific Plan Update also requires all development projects to use recycled urban water for the irrigation of landscapes, plazas, and playgrounds to reduce demand for potable water. All new projects shall provide purple pipes in the street adjacent to their property for future hookup to the citywide purple pipe network, and pay for Development Impact Fees as required in Article XVIII of the City's Zoning Code. All public improvements shall be constructed in accordance to the most recent edition of the Millbrae Public Works Standard Plans and Specifications.

Wastewater Service

The City provides sewer services throughout its jurisdiction and for Capuchino High School. Sewage is collected primarily in gravity flow lines supplemented by three pumping stations and force mains that convey flows to the Millbrae WTP within the limits of the Specific Plan Area. The Millbrae WPCP, also located within the Specific Plan Area, provides primary and secondary treatment for all sanitary sewage generated within the city limits and Capuchino High School. Wastewater line improvements within the Specific Plan Area would need to occur to provide adequate sewer services to properties in the Specific Plan Area as they develop. Required wastewater line improvements are shown on Figure 3-17.

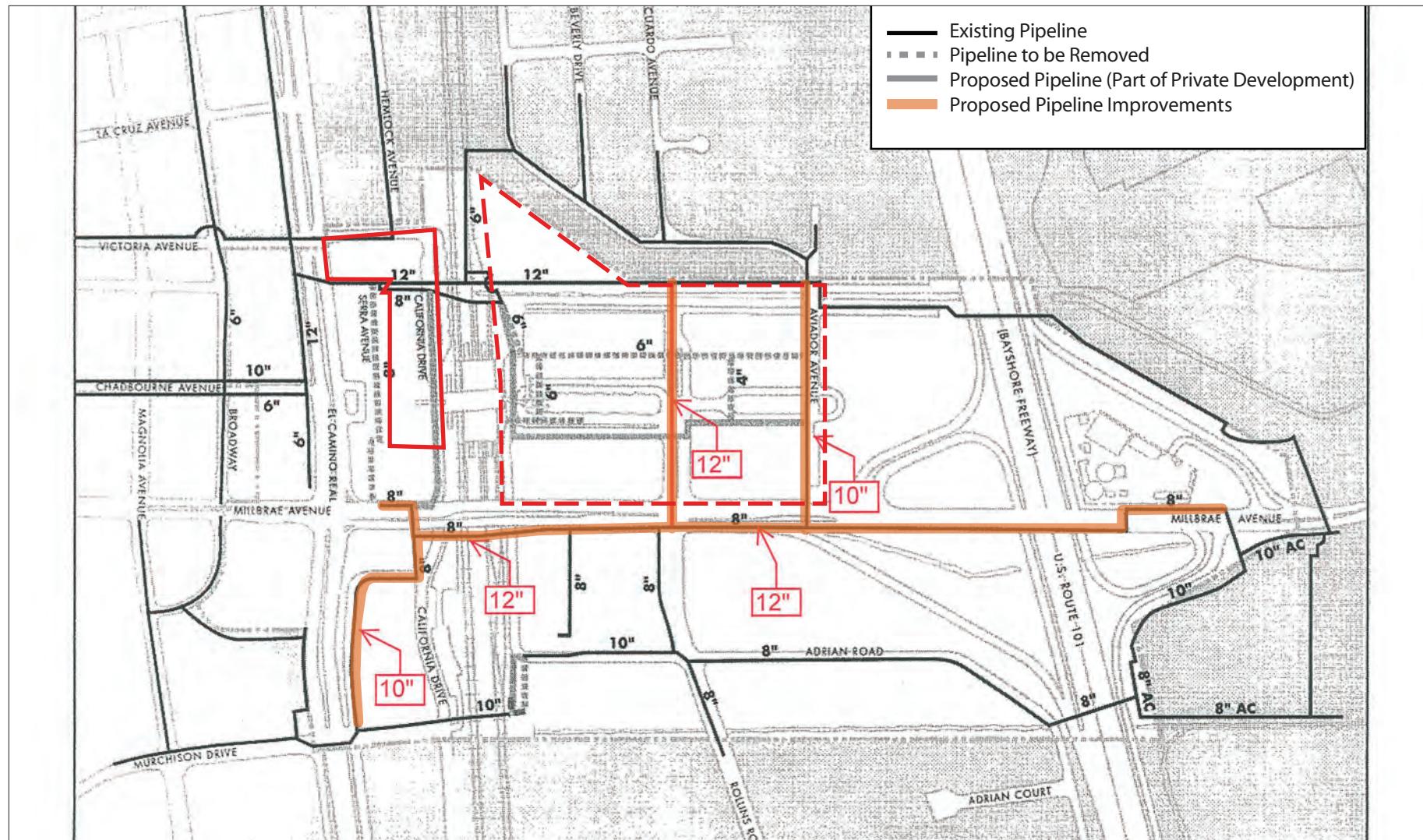
Stormwater

Stormwater in Millbrae is conveyed through three primary trunk lines consisting of storm drain pipes, open channels, and pump stations before discharging into the San Francisco Bay. The Specific Plan Area is bordered by the Highline Canal to the north and the El Portal Canal to the South. New storm drain collector systems in California Drive, along the western edge of the railroad tracks, and in the new roadway area along the southern edge of the BART bus parking area would be required. Existing storm drain lines will need to be extended or reconstructed within the public street areas or within new public utilities easements to serve the parcel areas. Private storm drain improvements would be required for individual onsite collection systems. Installation and maintenance of these improvements would be the responsibility of the individual parcel developers.

Solid Waste

South San Francisco Scavenger Company collects solid waste under franchise with the City. Millbrae's solid waste is processed at a transfer station at Oyster Point in South San Francisco and is transported from there to the Ox Mountain sanitary landfill site in Half Moon Bay. New development under the Specific Plan Update would be served by these existing services.

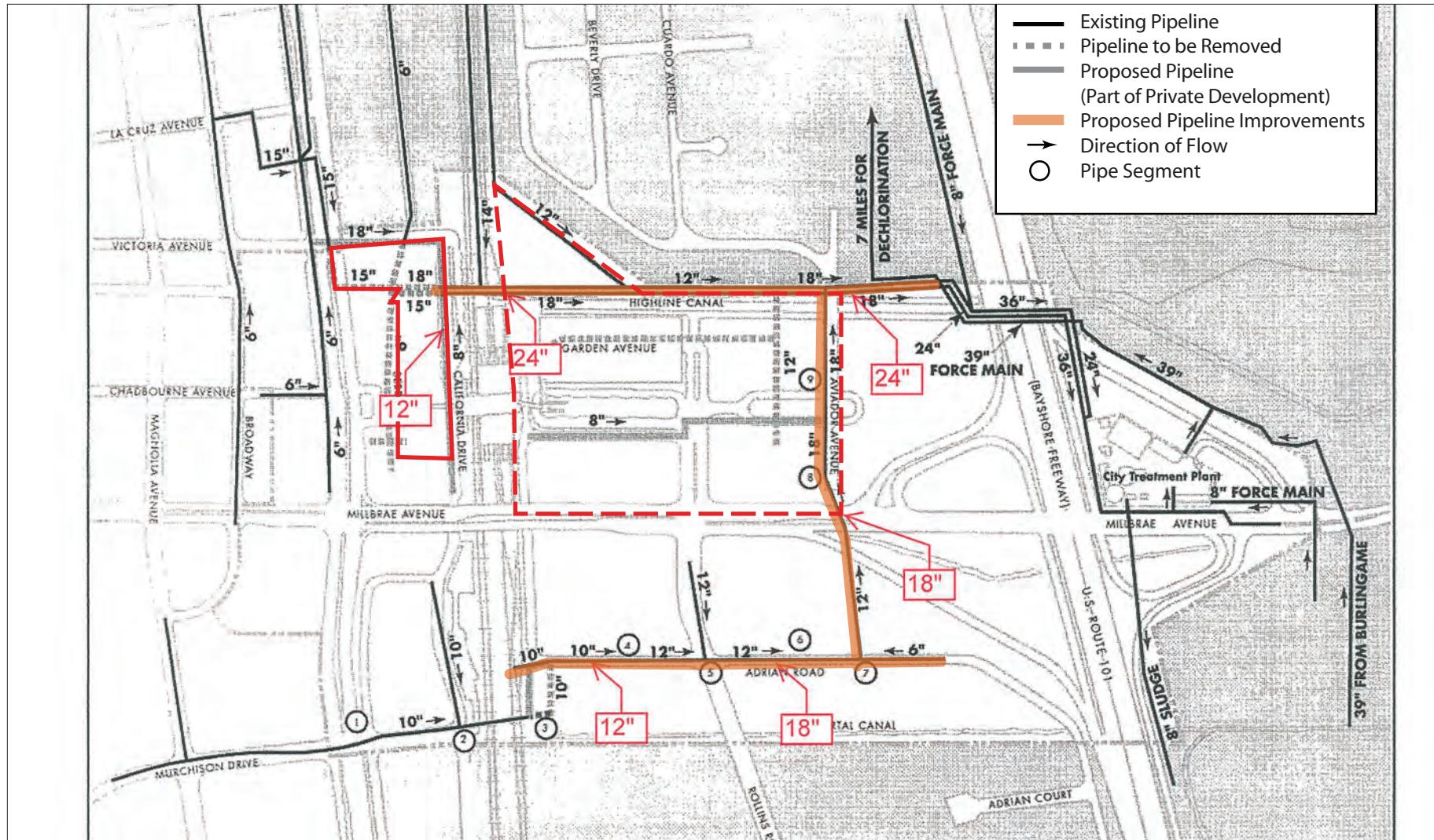
Millbrae residents participate in curbside recycling program for paper, aluminum, glass and plastic. The City operates a Source Reduction and Recycling program designed to meet state law requiring a 50 percent reduction of waste to landfills.



Source: PlaceWorks, 2015.

Figure 3-16

Specific Plan Update Water Line Schematic Improvements



Source: PlaceWorks, 2015.

 TOD #1 Boundary

 TOD #2 Boundary

Figure 3-17

Figure 3-17

PROJECT DESCRIPTION

Electricity, Natural Gas and Telephone

Electricity and gas are provided to the City by Pacific Gas and Electric (PG&E). The existing electrical system consists of overhead and underground facilities. Four sets of 115-kilovolt (kV) electrical transmission lines traverse the eastern portion of the project area in a north-south direction. Gas service is provided throughout the city through a system of underground gas mains. New underground electrical lines would be required to replace existing lines if realignment is required under future development. New development under the Specific Plan Update would continue to be served by PG&E on a case-by-case basis in accordance with Millbrae Municipal Code, Chapter 11.05 (Electrical Transmission Franchise) and Chapter 11.10 (Gas Distribution Franchise).

New underground electrical and gas lines would be required to replace existing lines if realignment is required under future development. Future lines could be located within the public right-of-way or in dedicated easements in a joint trench with electrical, gas, telephone, and cable utilities. All development projects in the Specific Plan Area would also be required to provide 2- to 4-inch conduits for fiber optic in the street in front of their properties, and pay for Development Impact Fees as required in Article XVIII of the City's Zoning Code.

3.2.4 OBJECTIVES OF THE PROPOSED SPECIFIC PLAN UPDATE

The primary intent of the proposed Specific Plan Update is to revise the 1998 Specific Plan to facilitate new Class A office, retail, hotel and residential development in proximity to the Millbrae Station to respond to changing market conditions and demographic shifts, while considering other planning goals, such as enhancing pedestrian mobility, bicycle circulation, and transit access. The City has developed the following project objectives that are meant to aid decision-makers in their review of the proposed Specific Plan Update, the alternatives to the proposed Specific Plan Update, and associated environmental impacts:

- Update and refine the vision for the Specific Plan Area for a 25-year buildout (2040) horizon that responds to ongoing changing market conditions and demographic shifts and citywide and regional multi-modal transportation goals.
- Ensure the Specific Plan Update will strengthen the City's economic base by supporting economic development, and enhance and revitalize commercial areas.
- Establish new goals and policies intended to facilitate the achievement of a 25-year buildout horizon for the Specific Plan Area.
- Ensure a Specific Plan Update that is consistent with the City's PDA designation by ABAG and MTC through the Bay Area's Regional FOCUS program, and therefore encourages high density development in close proximity to transit nodes that will help to reduce greenhouse gas emissions through a reduction in vehicle trips.
- Reconsider and designate the Specific Plan Area for appropriate new land uses, including eliminating underutilized industrial and non-retail land uses, to implement the updated vision of the Specific Plan Update.
- Plan for opportunities for increased use of transit, pedestrian and bicycles within the Specific Plan Area.
- Identify recommendations for circulation and physical improvements required to support a 25-year buildout horizon of the Specific Plan Area, all of which prioritize pedestrian mobility, bicycle access, and transit access.

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- Create updated building and design standards for new development that respond to changing market forces and demographic shifts, and facilitate multi-modal transportation in the Specific Plan Area.
- Create updated building and design standards that minimize the impact of new development on the character of surrounding residential neighborhoods.
- Provide an implementation strategy and conceptual financing plan for achieving the goals of the Specific Plan Update over a 25-year buildout the Specific Plan Area.
- Facilitate the redevelopment of the underutilized portions of the Specific Plan Area with a Class A office corridor south of Adrian Road, residentially-focused mixed-use development along El Camino Real, and a highly flexible mix of uses in the areas immediately surround the BART station to the west, east, and south.
- Permit and encourage hotels flexibly within all portions of the Specific Plan Area.
- Create policies that balance between minimization of vehicular parking to discourage auto use and foster a walkable and bikeable urban environment with ensuring project viability.

3.2.5 BUILDOUT OF THE PROPOSED SPECIFIC PLAN UPDATE

As shown above in Table 3-1 in Section 3.1.4, Buildout Projections, with implementation of the proposed Specific Plan Update the total buildout of the Specific Plan Area would result in the following:

- 1,653,340 square feet of office space
- 275,110 square feet of retail space
- 1,750 residential units
- 370 hotel rooms
- 4,630 residents¹⁶
- 8,100 employees

3.2.6 IMPLEMENTATION

Implementation of the proposed Specific Plan Update would involve the following:

- **General Plan Amendment.** The General Plan must be amended concurrently with the proposed Specific Plan Update. General Plan Amendments will ensure that land use designations and other pertinent policies are consistent between the proposed Specific Plan Update and the Millbrae General Plan 1998-2015. Potential changes include the following:
 - Update Policy LU 3.5 to reflect the revised vision and goals of the Specific Plan.
 - Update the General Plan Bicycle and Trail Routes Map.

¹⁶ As some of the proposed residential units would be studio and one-bedroom units accommodating single residents and couples without children, it is likely that a resident population of 4,630 is high; thereby allowing for a conservative analysis of potential environmental impacts.

PROJECT DESCRIPTION

- Revise Policy CIP-12 to show the updated list of street improvements that are identified in this Specific Plan.
- Update Map 4-2 (Millbrae Street Classification System) and Map 4-3 (Millbrae Transit System).
- **Zoning Ordinance Amendment.** The Zoning Ordinance must be amended concurrently with the proposed Specific Plan Update to ensure development intensities, heights, and other pertinent requirements are consistent between the proposed Specific Plan Update and the Zoning Ordinance. Adoption of the Specific Plan will require the following amendments to Article XVII (Millbrae Station Area Planned Development of “MSAPD” District) of the City’s Zoning Code:
 - Add a reference of the updated Specific Plan to Section 10.05.1709 Millbrae Station Area Specific Plan.
 - Update definitions under Section 10.05.1805 (Definitions) to add terminology defined in the Specific Plan.
- **Public Improvements.** The City would seek funding (e.g. taxes, grants, developer fees and permits, etc.) to implement the capital improvements necessary to implement the proposed Specific Plan Update.
- **Private Improvements and Developers.** Private developers would construct required infrastructure improvements and development allowed by the proposed Specific Plan Update.

3.2.7 PUBLIC SERVICES

Public service providers in Millbrae and San Mateo County that would serve the proposed Project include the following:

- The Central County Fire Department (CCFD), a joint powers and governmental agency, provides fire protection services to the City through a contractual agreement that benefits the Town of Hillsborough and the Cities of Burlingame and Millbrae. All future development and street improvement projects shall be subject to the CCFD’s and City’s review processes to ensure adequate access to public safety vehicles.
- The Millbrae Police Bureau, a division of the San Mateo County Sheriff’s Office, and the BART Police Department provide police protection services in the city and Specific Plan Area. All future development and street improvement projects shall be reviewed to ensure adequate access to public safety vehicles.
- The City’s parks are maintained by the Parks and Recreation Department.
- The city is within the boundaries of the Millbrae Elementary School District and San Mateo Union High School District.
- Telephone service for landlines would be provided by AT&T. Television service would be available from a number of providers, including Comcast, DISH®, and Direct TV.
- South San Francisco Scavenger Company collects solid waste under franchise with the City. Millbrae’s solid waste is processed at a transfer station at Oyster Point in South San Francisco and is transported from there to the Ox Mountain sanitary landfill site in Half Moon Bay.

3.2.8 REQUIRED APPROVALS

The proposed Specific Plan Update is a policy-level document and does not include any specific development proposals other than the proposed TOD #1 and TOD #2 projects that are evaluated in this Draft EIR. Therefore, the Specific Plan Update would be adopted solely by the Millbrae City Council. The Planning Commission and other decision-making bodies would review the proposed Specific Plan Update and make recommendations to City Council. While other agencies may be consulted during the adoption process, their approval is not required for the Specific Plan Update adoption. However, subsequent development under the Specific Plan Update may require approval of State, federal and responsible trustee agencies that may rely on the program-level analysis in this EIR for decisions in their areas of expertise. The EIR will evaluate the impacts related to the issuance of the following land use permits from the City of Millbrae:

- General Plan Amendment
- Zoning Ordinance Amendment
- Specific Plan Adoption
- Subdivision Map
- Development Agreement Approval
- Design Review approval
- Grading Permits
- Building Permits

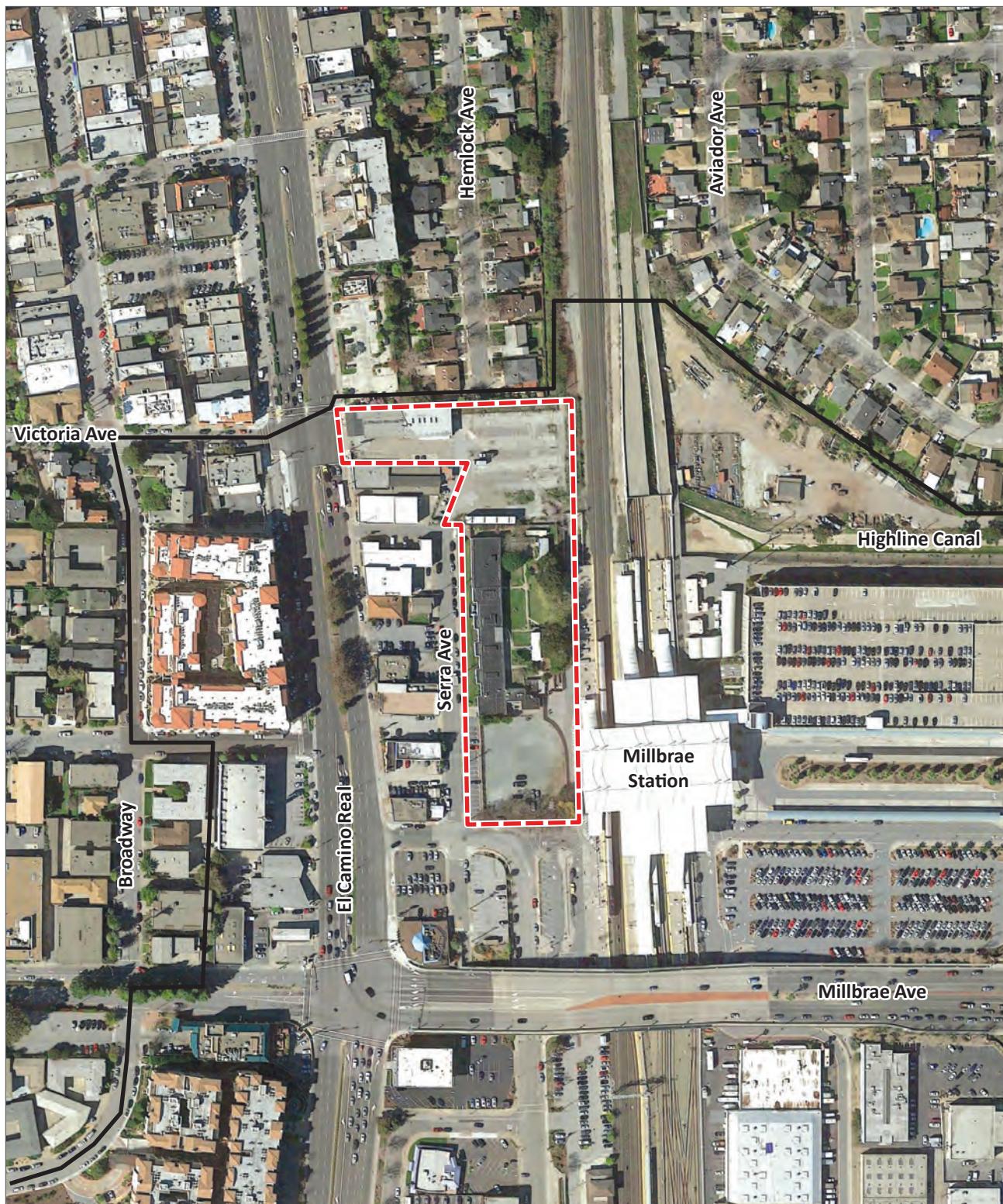
3.3 DESCRIPTION OF THE TOD #1 PROJECT

The proposed TOD #1 project proposes new mixed-use development on the Millbrae Serra Station properties (Applicant), which are located immediately west of the Millbrae Station. The mix of uses associated with the proposed TOD #1 project includes office, retail, high- to medium-density multi-family residential, underground parking, and an enclosed galleria retail corridor connecting Serra Avenue to the Millbrae Station platform. The project site plans shown in Figures 3-19 through 3-25 are conceptual for the purposes of conducting environmental review and do not represent final site plans, which would be refined during the City's project approval and design review process.

3.3.1 EXISTING CONDITIONS

3.3.1.1 TOD #1 LOCATION

The TOD #1 project site represents a portion of sub-area 1 in the 1998 Specific Plan. As shown on Figure 3-18, this project site is located south of the northern boundary of the Specific Plan Area, west of the Millbrae Station, east of El Camino Real and Serra Avenue, and north of Linden Avenue. The site is generally flat with elevation ranges from approximately 15 to 20 feet above mean sea level (MSL).



Source: Google Earth Pro, 2015.

0 500
Scale (Feet)

 Specific Plan Area Boundary
 TOD #1 Boundary

Figure 3-18
Aerial Photograph of the TOD #1 Location

3.3.1.2 EXISTING USES

Totaling 3.5 acres, the TOD #1 project site is currently developed with commercial land use and includes minimal landscaping. Table 3-7 lists each property by street address and Assessor's Parcel Number, and includes a brief description of the current on-site uses.

TABLE 3-7 **TOD #1 EXISTING ON-SITE USES**

Address	APN	Acres	Square Feet	Existing Uses
100 Serra Avenue	024-337-080	0.74	32,100	paved surface parking lot
150 Serra Avenue	024-337-090	1.16	50,400	Millbrae Serra Skilled Nursing Facility
190 El Camino Real	024-154-200	0.18	7,800	a one-story commercial building with cabinet shop and real estate company
200 El Camino Real	024-154-460	1.37	59,500	former Millbrae Lumber Company; a one-story showroom and warehouse on El Camino Real; paved parking area
No address	024337-10	0.09	4,000	Highland Canal easement

Notes: TOD = transit-oriented development, APN = Assessor's Parcel Number.

3.3.1.3 LAND USE AND ZONING

General Plan and Zoning Ordinance

As shown on Figures 3-4 and 3-5 above, the TOD #1 project site is within the Millbrae Station Area Specific Plan (MSASP) General Plan land use designation¹⁷ and Millbrae Station Area Planned Development (MSAPD) zoning district.¹⁸

TOD #1 Land Use Concept and Planning Zone

As shown on Figures 3-7 and 3-9, the TOD #1 project site is within the Transit-Oriented Development land use designation and planning zone. See Section 3.2.3.1, Land Use Concept, and 3.2.3.3, Development Standards (Planning Zones), for a description of these designations. As shown on Figure 3-10, the maximum height limits for this site range between 108 feet to 120 feet subject to the SFO ALUCP, and FAA and SFO approval.

¹⁷ Millbrae 1998-2015 General Plan, Map 3-4, Land Use Map. Adopted November 24, 1998.

¹⁸ Millbrae Municipal Code, Title 10, Planning and Zoning, Chapter 10.05 Zoning, Article XVII, Millbrae Station Area Planned Development or “MSAPD” District.

PROJECT DESCRIPTION

3.3.1.4 SURROUNDING USES

As shown on Figure 3-4 and 3-5 above, the TOD #1 project site is bound by General Commercial and Low Density Residential land use designations, and Commercial and Single Family zoning districts to the north, and MSASP land use designations and MSAPD zoning districts to the east, south and west.

The TOD #1 project site is located a few blocks southeast of Downtown Millbrae. To the east, the TOD #1 project site is bound by Caltrain's right-of-way and the Millbrae Station. South of the project site across Linden Avenue, parcels contain surface parking lots. To the west of the project site, nine parcels are located at 100 to 184 El Camino Real, on the east side of El Camino Real, west of Serra Avenue. These parcels contain one- and two-story commercial buildings and surface parking areas. These parcels are occupied by a variety of commercial uses, including, but not limited to, retail, restaurant, auto-related, salon, and health service businesses. The property at 186 El Camino Real is located adjacent to the TOD #1 project site, on the east side of El Camino Real and north of Isabel Alley. This property contains a one-story building that is occupied by a kitchen and bath showroom. To the north of the project site, there are single family homes, a fast-food restaurant (Taco Bell), and associated parking lot.

The western border of the TOD #1 project site is located along El Camino Real, which is the subject of the Grand Boulevard Initiative described above in Section 3.2.1.5, Grand Boulevard Initiative.

3.3.2 OBJECTIVES OF TOD #1 PROJECT

The primary intent of the proposed TOD #1 project is to develop a high-quality mixed-use development in the Specific Plan Area. In coordination with the City, the Applicant has developed the following project objectives that are meant to aid decision-makers in their review of the proposed project, the alternatives to the proposed project, and associated environmental impacts:

- Design and construct a project consistent with the intent of the proposed Specific Plan Update.
- Redevelop an underutilized property within the Specific Plan Area to provide a high-quality, high-density mixed-use project directly adjacent to the Millbrae Station that provides a well-designed and well-situated mixed-use development for current and future residents and employees desiring to reside and work in a transit friendly environment in Millbrae with convenient transit connectivity to the larger Bay Area.
- Build a project consistent with the City's PDA designation by the ABAG and the MTC through the Bay Area's Regional FOCUS program, which is intended to encourage high density new development in close proximity to transit nodes that will help to reduce greenhouse gas emissions through a reduction in vehicle trips.
- Redevelop an underutilized property within the Specific Plan Area with a commercially viable mixed-use development including up to Class A office, retail and high-density residential units for the City and its residents.
- Redevelop an underutilized property within the Specific Plan Area with mixed-use development within a half-mile of the City's primary gateway near Highway 101 for direct access to the San Francisco International Airport (SFO) and adjacent to the Millbrae Station to provide convenient access for residents and employees to utilize public transit.

- Design and construct a project that accommodates the needs of transit service providers to ensure safe and reliable transit access.
- Design and construct a project that facilitates multi-modal access and emphasizes connections to transit.
- Design and construct a high-quality mixed-use development that minimizes the impact of new development on the character of surrounding residential neighborhoods and adds to the visual character of El Camino Real. Activate public open spaces and streets by fronting them with pedestrian-friendly ground floor design and active retail uses.
- Design and construct a project that provides publicly accessible open spaces.
- Achieve sustainable aspects of construction through current green building practices.
- Assemble additional parcels within the TOD #1 project site to create a gateway opportunity connecting the Millbrae Station to the downtown.

3.3.3 BUILDOUT OF TOD #1 PROJECT

As shown above in Table 3-1 in Section 3.1.4, Buildout Projections, the construction of the proposed TOD #1 project would result in the following:

- 267,000 square feet of office space
- 32,000 square feet of retail space
- 500 residential units
- 1,325 residents
- 1,148 employees

3.3.4 PROJECT COMPONENTS OF TOD #1 PROJECT

3.3.4.1 IMAGE AND CHARACTER

As shown on Figure 3-19, the proposed TOD #1 project includes one main building with ground floor retail and office uses and three residential towers over three levels of subterranean parking and an enclosed galleria retail corridor connecting Serra Avenue to the Millbrae Station platform.

Building Design and Height

The total lot area is 153,675 square feet and the total buildable area is 121,688 square feet. The proposed building would have a floor area ratio (FAR) of 5.74 and the residential density would be 143 units per acre.

The project's three towers, ranging from eight to ten stories tall, are described in the text and illustrated on the figures as Building A, Building B and Building C. The proposed setbacks and height limits for each tower are listed by building in Table 3-8 and illustrated on Figure 3-19 and Figure 3-20, respectively. As shown on Figure 3-21, the proposed heights range from up to 121 to 136 feet, which, as previously stated, are subject to the SFO ALUCP,

PROJECT DESCRIPTION

and FAA and SFO approval. Figure 3-22 shows the proposed project's east elevations from Railroad Avenue and west elevations from Serra Avenue and El Camino Real in relationship to the surrounding land uses.

TABLE 3-8 TOD #1 PROJECT BUILDING DESIGN PARAMETERS

Building	Building Levels	Height	Setback
A	8	121 feet	10 feet (see Figure 3-20)
B	10	136 feet	10 feet and 5 feet (see Figure 3-20)
C	10	135 feet	10 feet, 5 feet, and 2 feet (see Figure 3-20)

Notes: TOD = transit-oriented development; n/a = not applicable

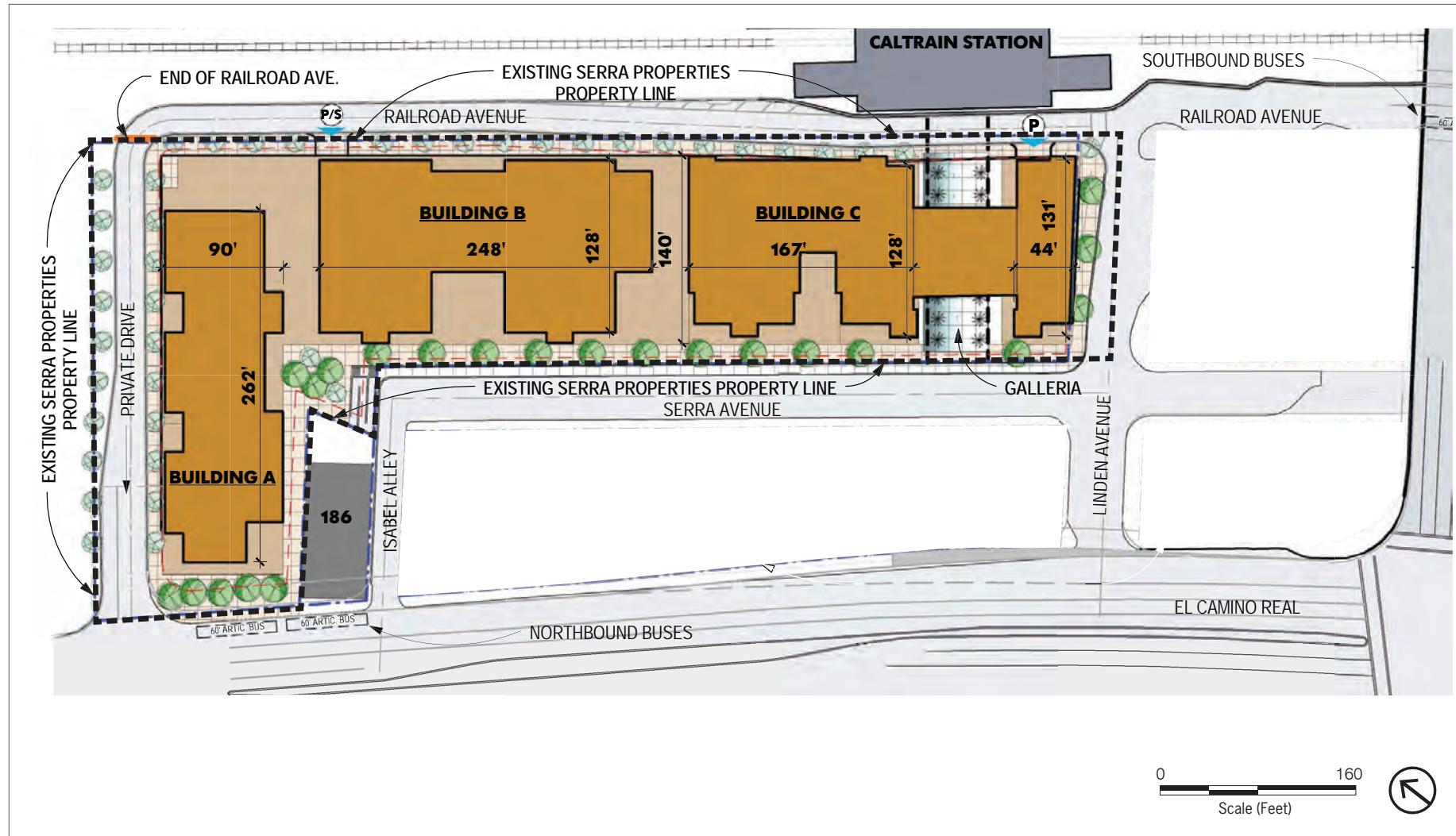
Streetscape and Landscape Design

The project would install streetscape features along the project site perimeter, including street lights, sidewalk treatments, trees, planters, landscaping, and street furniture. These features would be part of a unifying streetscape design to reinforce the character of the blocks and neighborhood. Landscaping features are illustrated on Figure 3-23. As many as 60 trees, including street trees, would be planted throughout the site. The street tree types will be determined according to the Millbrae Municipal Code Chapter 8.60, Millbrae Tree Protection and Urban Forestry Program during the City's design review process.¹⁹

Lighting

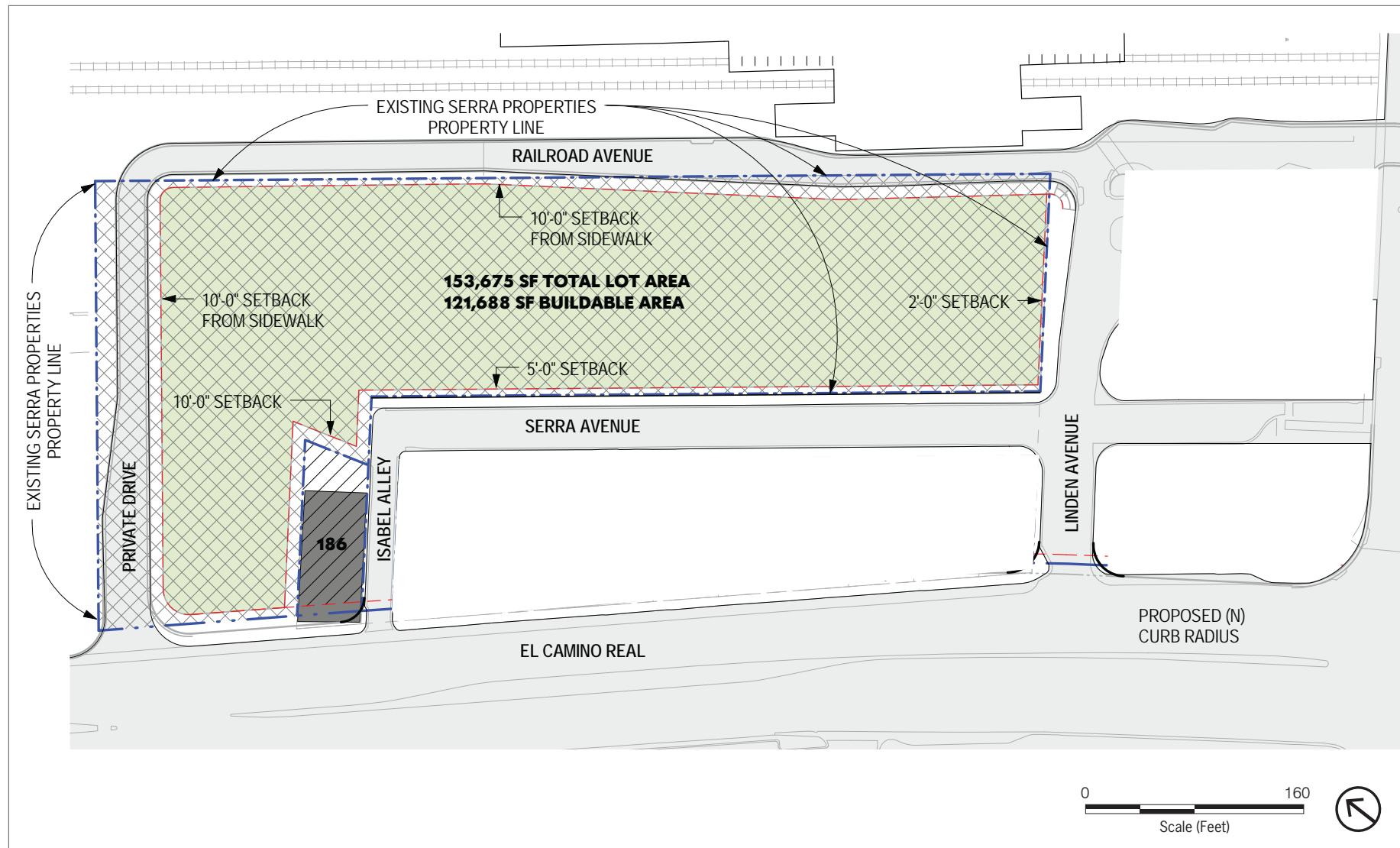
The source, intensity, and type of exterior lighting for the project site would be typical for orientation and safety needs. All on-site lighting would be low-level illumination, downward facing and shielded to reduce light spillover or glare. In landscaped and paved areas, light sources will be concealed. All exterior surface and above-ground mounted fixtures will be sympathetic and complementary to the architectural theme.

¹⁹ Millbrae Municipal Code Title 8 (Public Works), Chapter 8 (Millbrae Tree Protection and Urban Forestry Program).



Source: Millbrae Serra Station LLC, December 5, 2013.

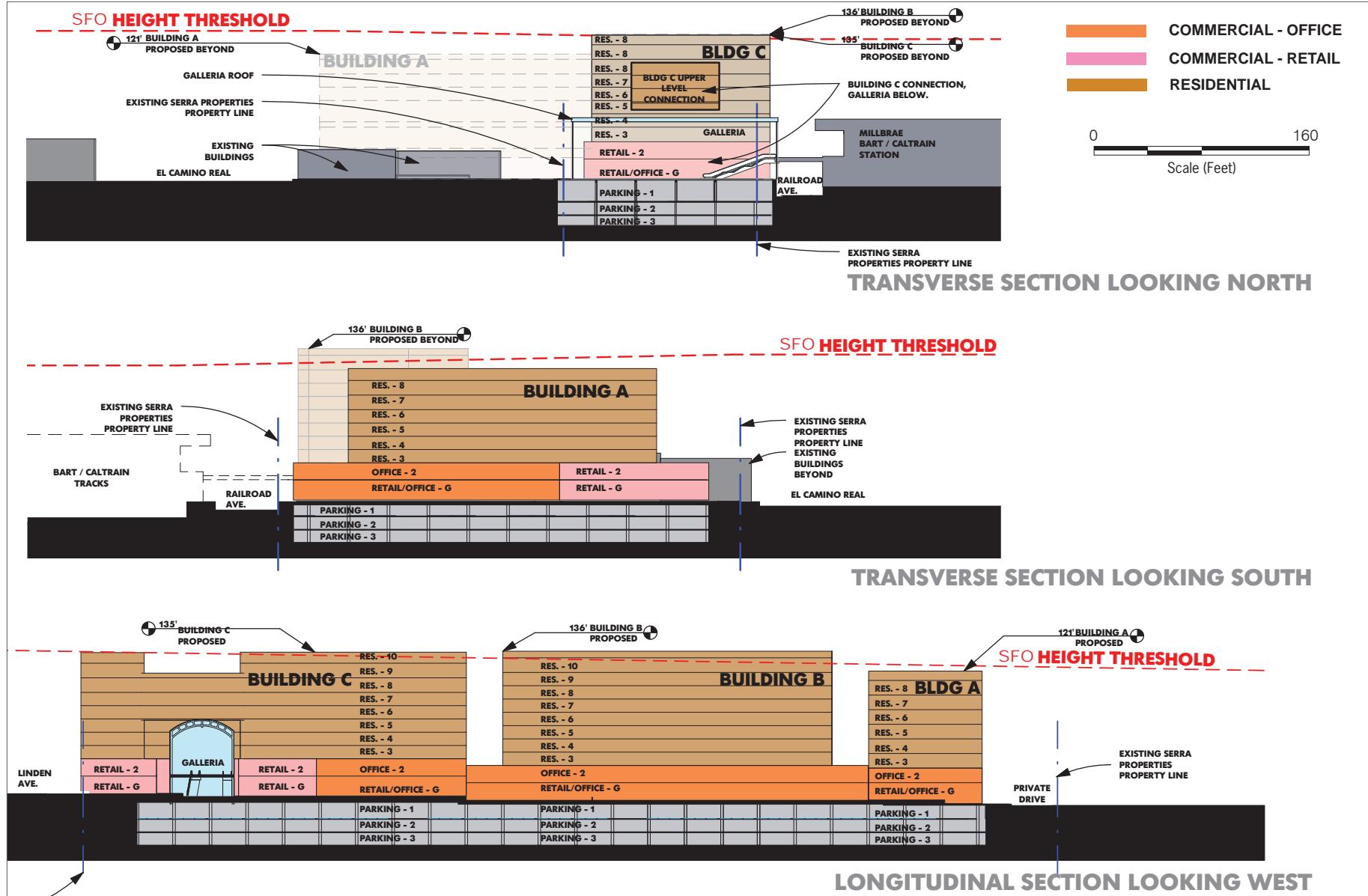
Figure 3-19
TOD #1 Site Plan



Source: Millbrae Serra Station LLC, December 5, 2013.

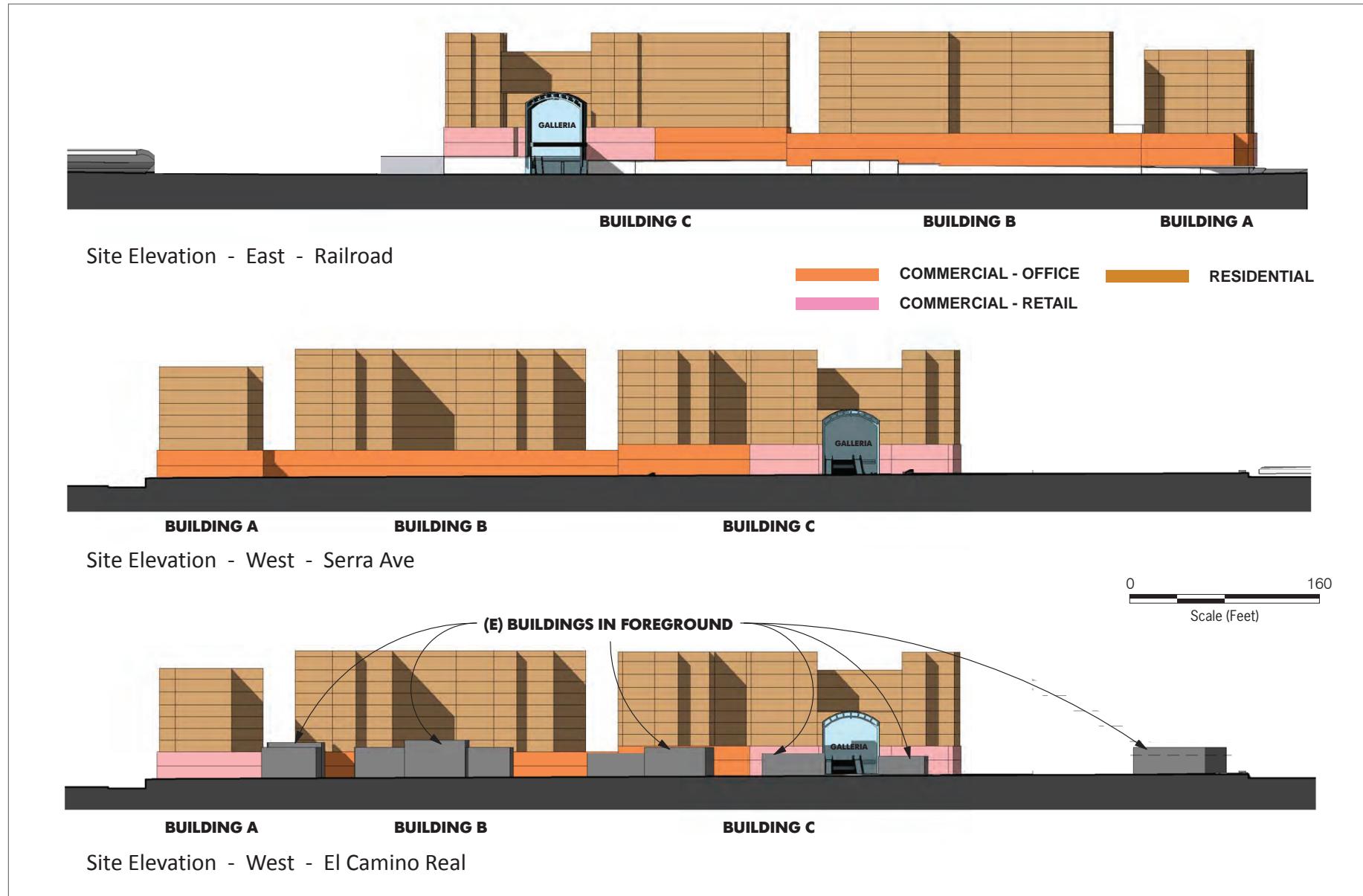
Figure 3-20
TOD #1 Setback Plan

PROJECT DESCRIPTION



Source: Millbrae Serra Station LLC, December 5, 2013.

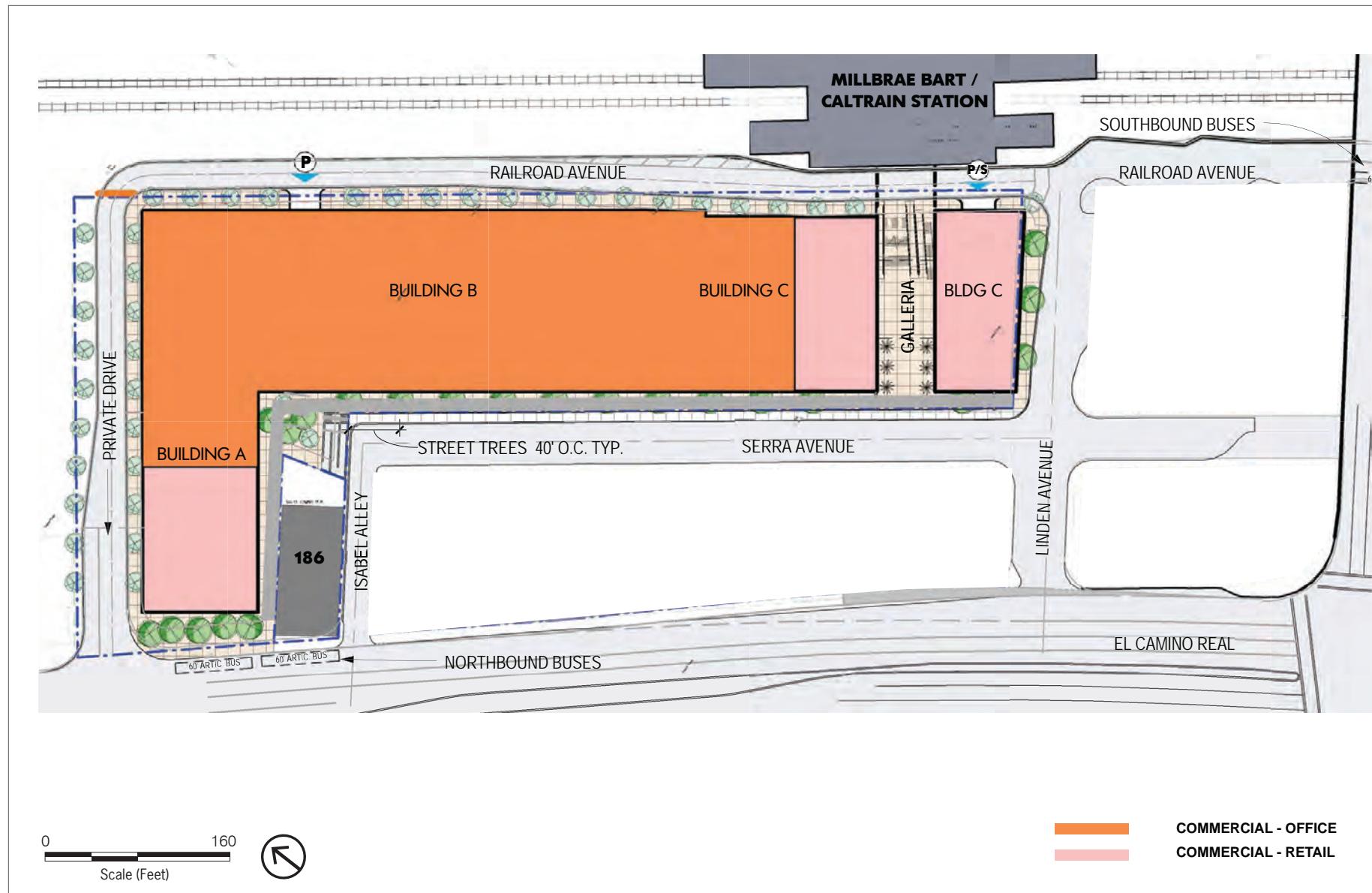
Figure 3-21
TOD #1 Building Sections

PROJECT DESCRIPTION


Source: Millbrae Serra Station LLC, December 5, 2013.

Figure 3-22

TOD #1 Elevations

PROJECT DESCRIPTION


Source: Millbrae Serra Station LLC, December 5, 2013.

Figure 3-23
TOD #1 Landscape Plan

PROJECT DESCRIPTION

3.3.4.2 OFFICE

The proposed project includes 267,000 square feet of Class A²⁰ office space on the first and second floors. Parking would be provided on all parking levels and would be accessed from the limited-access driveway off of El Camino Real (described below in Section 3.3.4.5, Circulation and Access). Offices uses are anticipated to operate during normal business hours (e.g. 8:00 am to 5:00 pm) with limited after-hours access for cleaning/maintenance crews.

Based on an average of 1 job per 250 square feet for office, the proposed project would generate up to 1,068 employees.

3.3.4.3 RETAIL

The proposed project includes 32,000 square feet that could include a mix of small-format destination retail (e.g. drug store, restaurant), and neighborhood-serving retail (e.g. dry cleaning, mailing/shipping service, specialty food market) space on the first and second floors of Building A and Building C. As shown on Figure 3-21, the enclosed galleria retail corridor would accommodate the desired foot traffic to and from the access points at the Millbrae Station. Parking would be provided on all of the parking levels and would be accessed from Railroad Avenue (described below in Section 3.3.4.5, Circulation and Access). Retail uses are anticipated to operate during normal business hours (e.g. 8:00 am to 5:00 pm) with limited after-hours access for cleaning/maintenance crews.

Based on an average of 1 job per 400 square feet for retail, the proposed project would generate up to 80 employees.

3.3.4.4 RESIDENTIAL

The proposed project would involve construction of 500 market-rate rental apartment units in three towers above the retail and office uses as shown on Figure 3-21. The residential area would comprise approximately 600,000 square feet of space. The project would include 50 one-bedroom, 400 two-bedroom, and 50 three bedroom apartment units ranging in size from 750 square feet (smallest one-bedroom unit) to 1,200 square feet (largest three-bedroom unit). Based on an average household size of 2.65 persons,²¹ it is assumed the proposed project would house approximately 1,325 residents. It is anticipated that residents of the project would be drawn largely from Millbrae and other communities in the San Francisco Bay Area.

The site would also contain 12,500 square feet of common open space (25 square feet per unit) and 33,500 square feet of community space. The residences would be located on the third through eighth stories in Building A and

²⁰ These buildings represent the highest quality buildings in their market. They are generally the best looking buildings with the best construction, and possess high quality building infrastructure. Class A buildings also are well-located, have good access, and are professionally managed. As a result, they attract the highest quality tenants and also command the highest rents.

²¹ Population is based on 2.65 persons per dwelling units consistent with U.S. Census Bureau's 2005-2009 American Community Survey 5 year estimates.

the third through tenth stories in Buildings B and C. Residential parking would be provided on all parking levels and would apply an unbundled and managed approach.²²

Recreational amenities could include an athletic club, featuring both an exercise room and a yoga room, and an outdoor seating area.

3.3.4.5 CIRCULATION AND ACCESS

Vehicular

Figure 3-24 shows the road layout for the proposed TOD #1 project. As shown on this figure, the proposed vehicular circulation and access to the project site would occur on the existing Serra Avenue and Linden Avenue, but would also include two new roadways. The new roadways are shown as Railroad Avenue along the site's eastern border with the railroad tracks and a limited-access driveway to the north. The two main access points to the parking garage are located on Railroad Avenue, which would be accessed from Linden Avenue off of El Camino Real and from California Drive. Millbrae Avenue is elevated adjacent to the project site in order to provide an overpass over the railroad tracks; thus, no direct access would occur from Millbrae Avenue.

The limited-access driveway would connect to Railroad Avenue for access to the parking garage and would be restricted to use by office employees only. This limited-access driveway would serve as a buffer between the project and the single-family homes to the north. As shown on Figure 3-24, a 16-foot buffer would be provided between the roadway and the single-family homes and as shown on Figure 3-23, the buffer would be landscaped. The 24-foot to 36-foot roadway and 10-foot sidewalk would also serve as a buffer between these two uses. In addition, the private driveway would provide an easement for city utilities that will need to be rerouted to accommodate the proposed project.

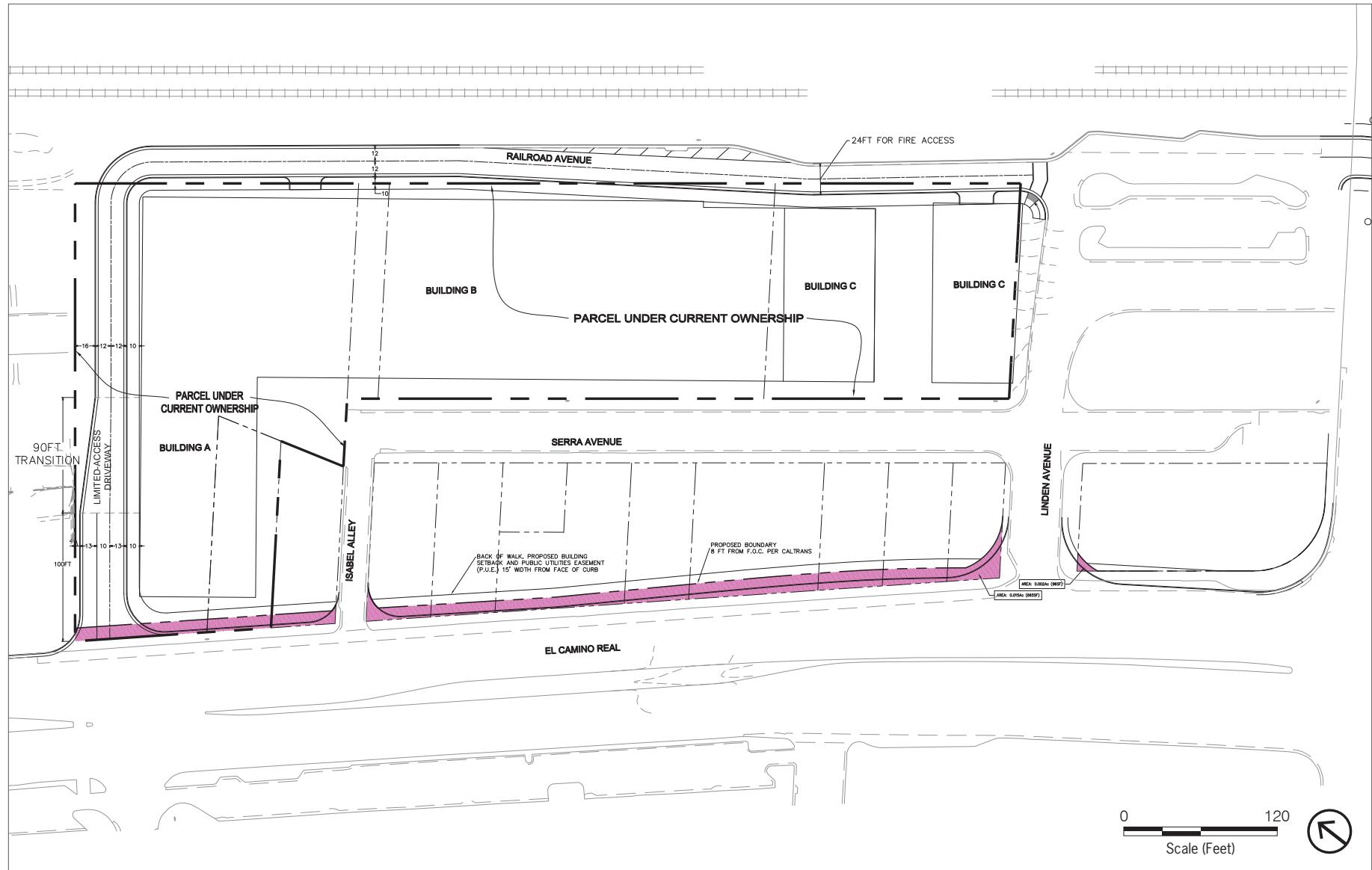
Emergency Response Vehicles

The proposed project would be required to comply with the minimum specifications in Municipal Code Chapter 9.30, which includes the California Fire Code, adopted by reference and local amendments.²³ As shown on Figure 3-24, emergency response vehicles would have access to the project site on all sides. The new roadways (Railroad Avenue and the limited-access driveway) would include two 12-foot travel lanes to accommodate emergency vehicle access.

²² Parking spaces would be rented or sold separately (i.e. unbundled), rather than automatically including them with building space. In other words, a residential unit would not automatically (i.e. bundled) come with a parking space as part of the rent or lease.

²³ Millbrae Municipal Code, Title 9, Building Regulations, Chapter 9.30, Fire Code.

PROJECT DESCRIPTION



Source: Millbrae Serra Station LLC, December 5, 2013.

Figure 3-24
TOD #1 Road Layout

Pedestrian and Bicycle

As shown on Figure 3-23, the proposed project includes 10-foot sidewalks with landscaping around the perimeter of the buildings. Per the Grand Boulevard Initiative, the 15-foot setback on El Camino Real would create a larger sidewalk. Pedestrian access is also provided via an enclosed galleria retail corridor connecting Serra Avenue to the Millbrae Station platform. While the project does not propose any new bicycle lanes or routes, bikes would share the same on-site roads and access points with vehicles. The proposed project would provide sheltered bicycle lockers or storage rooms within the building for residents and employees. Outdoor bicycle racks that are compatible with the most common locking devices would be provided at each building entrance. On site pedestrian and bicycle facilities would include lighting for safety.

3.3.4.6 PARKING

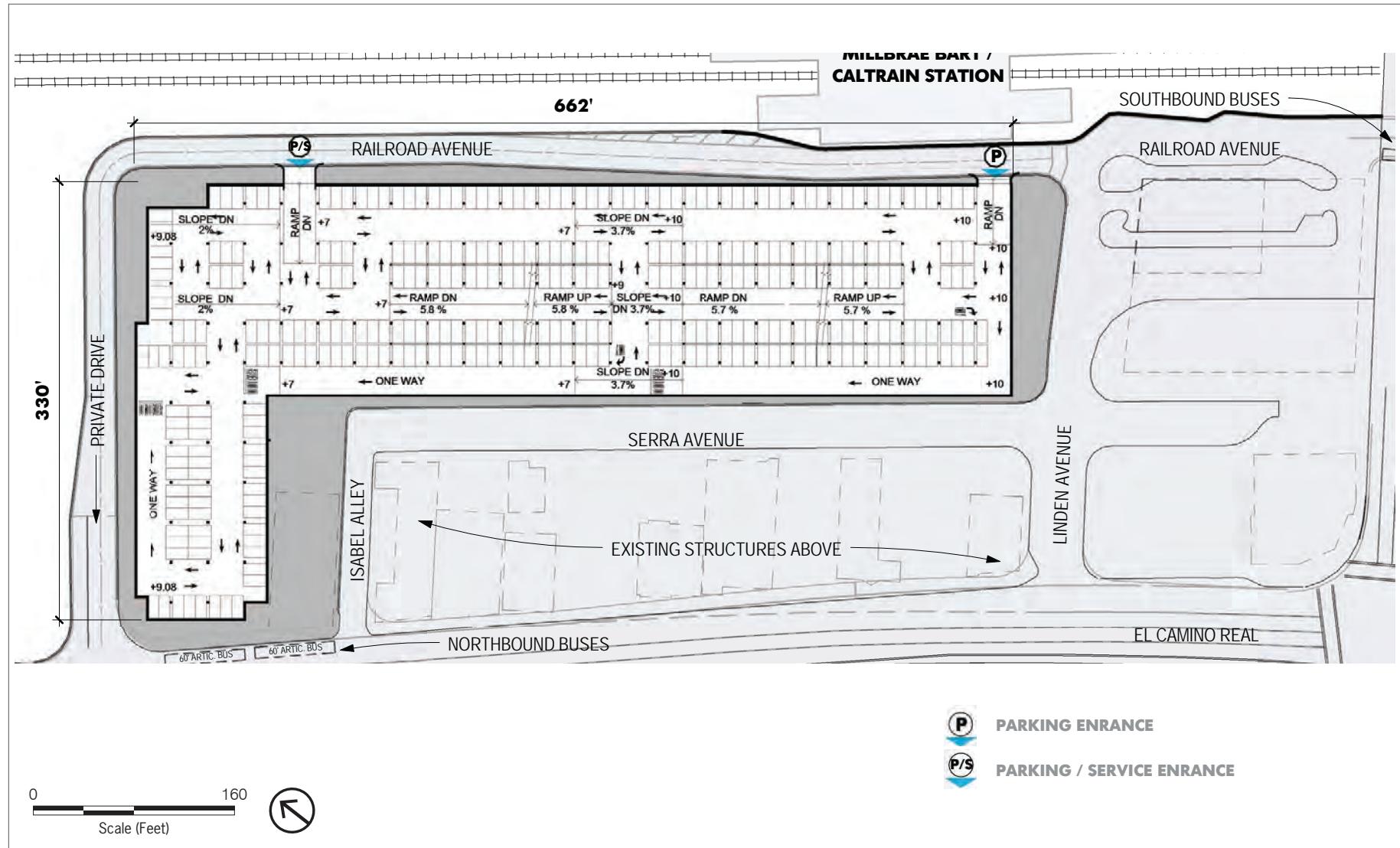
The proposed TOD #1 project includes three levels of parking structure with 1,067 parking stalls with required accessible spaces meeting the Americans with Disabilities Act (ADA) standards. A typical parking plan is shown on Figure 3-25. Parking level one includes 358 parking stalls, parking level two includes 358 parking stalls, and parking level 3 includes 351 parking stalls. Table 3-9 shows the total parking stalls by land use for vehicles and bicycles.

TABLE 3-9 TOD #1 PROJECT PARKING SUMMARY

Land Use	Size	Vehicular Parking Stalls	Long-term Bicycle Parking Stalls	Short-term Bicycle Parking Stalls
Office	267,000 sf	535	27	13
Retail	32,000 sf	32	22	6
Residential	500 units	500	500	50
Total		1,067	624	69

Notes: TOD = transit-oriented development, sf = square feet

Per the Specific Plan Update, parking for both long-term and short-term bicycle parking would be required as described in Table 3-6 in Section 3.2.3.3 Development Standards above.

PROJECT DESCRIPTION


Source: Millbrae Serra Station LLC, December 5, 2013.

Figure 3-25
TOD #1 Parking Plan

PROJECT DESCRIPTION

Two levels of parking would be below grade, and the third level would create a raised podium along Serra Avenue. The parking garage would be accessed by Railroad Avenue. The proposed TOD #1 project proposes less parking than would be required in the 1998 Specific Plan and considers the following parking management strategies to reduce parking demand:

- Unbundled parking, along with a Residential Permit parking program, would be intended to protect surrounding residential neighborhoods. With unbundled parking, the cost of parking is not included in real estate costs and tenants pay separately for parking spaces.
- Paid parking with congestion-priced parking.
- Shared parking between daytime office and evening retail and housing uses.
- Parking “cash out,” under which employers would provide subsidized parking or transit tickets for employees and would provide the option of money in lieu of parking.
- Improved transit, including the relocation of buses to the west side of the rail line and on El Camino Real.
- Enhanced bicycle and pedestrian access to support non-vehicular means of transportation.

3.3.5 SITE PREPARATION AND CONSTRUCTION

3.3.5.1 DEMOLITION, SITE PREPARATION AND GRADING

The Applicant proposes to demolish the existing buildings, parking lots, and remove all the vegetation that is currently on the project site.²⁴ Demolition and construction would occur in four phases over an approximate 4-year time period. The demolition time period for each construction phase would take place over a period of approximately one month, beginning as early as January 2016. Site preparation and grading would be completed over a four to six-month period during each phase. Demolition debris would be off-hauled for disposal in accordance with the Municipal Code Section 9.35.010.²⁵ Debris to be hauled would include approximately 30 trees, 36,000 tons of building demolition debris, 48,000 tons of asphalt/concrete material, and 98,000 cubic yards of grading and soil off-haul. Typical equipment to be used for demolition and site preparation would include backhoes, excavators, concrete saws, skid steer loaders, graders, rubber-tired dozers, scrapers, and water trucks.

During the building construction phases, which would occur over the 4-year period, the project construction would be comprised of 500 residential units, 32,000 square feet of retail space, 267,000 square feet of office space, an approximately 485,000-square foot parking garage, 30,000 square feet of paved parking lots and up to one acre of hardscape (e.g. roadways, curb, gutters, planters, seat walls, etc.). Pile driving would occur for approximately three to six months during the building construction phase. Typical equipment to be used for construction of the project would include forklifts, backhoes, a crane, loaders, aerial lifts, a generator, welders, cement mixers, rollers, and a pavers, and air compressors.

²⁴ The demolition and construction details in this EIR are calculated estimates based on the project’s preliminary site plans and are used for environmental review purposes only. The exact time frames and construction equipment will be determined as the project fully matures during the project approval process.

²⁵ Millbrae Municipal Code, Title 9, Building Regulations, Chapter 9.35, California Green Building Code, Section 9.35.010 Adoption of the California Green Building Code Title 24, Part 6, 2013 Edition.

PROJECT DESCRIPTION

3.3.5.2 UTILITIES

Water Supply and Conservation

Water mains in the development area range from 6 to 12 inches in diameter, and are composed of asbestos-concrete and cast iron. Many of the water mains in the older parts of the city, such as the station planning area, are worn out and need to be replaced. The proposed project is within an area of Millbrae that has very corrosive soils, therefore, the City requires ductile iron pipe for proposed water lines. The proposed development will leave the existing 8-inch water service in Serra Avenue and no new water infrastructure should be required.

A turnout vault and meter is located on El Camino Real near Victoria Avenue at the City connection to the San Francisco Water Department system. The vault has space available for an additional meter to increase the fire flow and reliability in the development area. There are also water lines in Isabel Alley and Serra Avenue, which continue separately through the lumber yard to the east. The service in Serra Avenue is connected at Millbrae Avenue.

The project incorporates a number of features meant to conserve water used for on-site irrigation. Water conserving features include an automatic “smart” irrigation controller with rain-sensor, low precipitation/low angle irrigation spray heads, low volume drip tubing installed below mulch, low water consuming plants, soil moisture retention techniques, and mulching to reduce evapotranspiration from the root zone. A separate system using recycled gray water for non-potable uses may be incorporated on this project site.

Wastewater Service

A sanitary sewer line crosses El Camino Real and then runs down Isabel Alley and through the existing lumber yard to the east. There is a sanitary sewer line in Serra Avenue, which flows from south to north and connects to the sewer from Isabel Alley. There is also a sanitary sewer line located in El Camino Real, which is located along the eastern edge of the road and drains from south to north and connects to the sewer line, which drains along Isabel Alley. Sanitary sewer flows from the Millbrae Station are pumped along Linden Avenue to the service in Serra Avenue.

The sanitary sewer lines in Serra Avenue, Isabel Alley and the utility corridor through the lumber yard would remain. A new 6-inch service line would be needed in Victoria Avenue to the north to redirect the 6-inch line that drains south from Hemlock Avenue. A new 8-inch service in Railroad Avenue may still be required. Both of the new lines would connect to the existing 12-inch and 18-inch trunk lines that cross under the rail tracks and run parallel to Highline Canal.

Stormwater

The proposed redevelopment area is located near the downstream end of the of Millbrae's central portion of the storm drain system. The main city storm drain trunk line (4-foot by 11-foot box culvert) flows through the development area (near the north end of the site) and under the Caltrain/BART rail tracks (via 8-foot by 54-inch culvert pipes) into Highline Canal to the east of the development.

The proposed project includes improvements to the existing storm drain system by relocating the existing 4-foot by 11-foot box culvert that cuts through the TOD #1 project site and adding a new surface drainage inlet structure in the north corner of the site. The relocated box culvert and the new 4-foot by 11-foot box culvert from the new inlet structure would be connected to the existing culverts under the BART/Caltrain railroad tracks. See Figure 4.8.3 in Chapter 4.8, Hydrology and Water Quality, of this Draft EIR.

Solid Waste Service Access

Front load dumpster bins would be stored in the trash rooms of each of the buildings. Trash chutes would be provided on each floor. Dumpsters would be transported by lift to the trash enclosure location on the east side of Building B off of Railroad Avenue for pick up by the waste management company on trash day.

Electricity and Natural Gas

Electricity and gas are provided to the project site by PG&E. The existing electrical system consists of overhead and underground facilities. Serra Avenue and Garden Lane would be removed as part of the proposed TOD #1 project's development and new underground electrical and gas lines would be required to replace existing lines located in these streets. In addition, an on-site co-generation plant would be installed in the project's proposed building and would serve as a central plant to provide hot water and power to all the buildings on the project site.

3.3.6 PUBLIC SERVICES

The same public service providers described in Section 3.2.7, Public Services, above will provide services to the proposed TOD #1 project.

3.3.7 REQUIRED APPROVALS

The proposed TOD #1 project would require the following approvals from the City:

- General Plan and Zoning Amendment
- Subdivision Map
- Conditional Use Permit
- Development Agreement
- Demolition, Grading and Building permits

This project will also require a number of approvals from other agencies, including, but not limited to:

- San Francisco Regional Water Quality Control Board
- City/County of Association of Governments
- Federal Aviation Administration
- Caltrain
- San Francisco Bay Area Rapid Transit

PROJECT DESCRIPTION

3.4 DESCRIPTION OF THE TOD #2 PROJECT

The proposed TOD #2 project proposes a mixed-use project for a BART-owned site that is located immediately east of the Millbrae Station. The project would include office, retail, high- to medium-density multi-family residential, and hotel uses. This project also proposes a new surface parking lot for BART passengers on a parcel that is currently used as the City's storage yard. In addition, the proposed TOD #2 project would relocate the bus intermodal facility and existing surface parking spaces. The project site plans shown in Figures 3-27 through 3-36 are conceptual for the purposes of conducting environmental review and do not represent final site plans, which would be refined during the City's project approval and design review process.

3.4.1 EXISITING CONDITIONS

3.4.1.1 TOD #2 PROJECT LOCATION

The TOD #2 project site is referred to as sub-area 5, 6 and 8 in the 1998 Specific Plan. As shown on Figure 3-26, this project site is located in an area north of Millbrae Avenue, east of the railroad tracks, south of the Highline Canal and Bayside Manor neighborhood and west of Aviador Avenue.

3.4.1.2 EXISTING USES

Totaling 9.7 acres, the project site is made up of nine properties developed with BART, BART parking and the City's storage yard. The following lists each property by street address and Assessor's Parcel Number, and identifies the current on-site use.

- 200 Millbrae Avenue, APN 024-108-270 (The western portion is not part of the project site.)
- 300 Millbrae Avenue, APN 024-180-300 (The northern portion is not part of the project site.)
- 310 Millbrae Avenue, APN 024-181-010
- 55 Aviador Avenue, APN 024-181-020
- 165 Aviador Avenue, APN 024-180-210
- 175 Aviador Avenue, APN 024-180-220
- 105 Garden Lane, APN 024-180-200
- 109 Garden Lane, APN 024-108-190
- 115 Garden Lane, APN 024-180-180
- 185 Aviador Avenue, APN 024-174-300 (City Storage Yard)



Source: Google Earth Pro, 2015.

0 500
Scale (Feet)



Millbrae Station Area Specific Plan Boundary



TOD #2 Boundary

Figure 3-26
Aerial Photograph of the TOD #2 Location

PROJECT DESCRIPTION

3.4.1.3 LAND USE AND ZONING

General Plan and Zoning Ordinance

As shown on Figures 3-4 and 3-5 above, the TOD #2 project site is within the Millbrae Station Area Specific Plan (MSASP) General Plan land use designation²⁶ and Millbrae Station Area Planned Development (MSAPD) zoning district.²⁷

TOD #2 Land Use Concept and Planning Zone

As shown on Figures 3-7 and 3-9, the TOD #2 project site is within the Transit-Oriented Development land use designation and planning zone. See Section 3.2.3.1, Land Use Concept, and 3.2.3.3, Development Standards (Planning Zones) for a description of these designations. As shown on Figure 3-10, the maximum height limits for this site range between 35 feet to 100 feet subject to the SFO ALUCP, and FAA and SFO approval.

3.4.1.4 SURROUNDING USES

As shown on Figure 3-4 and 3-5, the TOD #2 project site is bound by General Commercial and Low Density Residential land use designations, and Commercial and Single Family zoning districts to the north, and MSASP land use designations and MSAPD zoning districts to the east, south and west.

To the north, the TOD #2 project site abuts the Highline Canal and the Bayside Manor residential neighborhood. To the east, the TOD #2 project site abuts a lot controlled by SFO known as sub-area 7 in the 1998 Specific Plan that is highly disturbed and routinely used for temporary construction staging and other temporary storage uses. To the southeast of the project site, a gas station is located on the north side of Millbrae Avenue. To the south, the TOD #2 project site is bound by Millbrae Avenue. Across Millbrae Avenue to the south are various businesses with surface parking lots, including a gas station, convenience store, and fast-food restaurants. To the west, the TOD #2 project site is bordered by the Millbrae Station and tracks.

3.4.2 OBJECTIVES OF TOD #2 PROJECT

The primary intent of the proposed TOD #2 project is to develop a high-quality, mixed-use development in the Specific Plan Area. In coordination with the City, the Applicant has developed the following project objectives that are meant to aid decision-makers in their review of the proposed project, the alternatives to the proposed project, and associated environmental impacts:

- Design and construct a project consistent with the intent of the proposed Specific Plan Update.

²⁶ Millbrae 1998-2015 General Plan, Map 3-4, Land Use Map. Adopted November 24, 1998.

²⁷ Millbrae Municipal Code, Title 10, Planning and Zoning, Chapter 10.05 Zoning, Article XVII, Millbrae Station Area Planned Development or “MSAPD” District.

PROJECT DESCRIPTION

- Redevelop an underutilized property within the Specific Plan Area to provide a high-quality, high-density mixed-use project directly adjacent to the Millbrae Station that provides a well-designed and well-situated mixed-use development for current and future residents and employees desiring to reside and work in a transit friendly environment in Millbrae with convenient transit connectivity to the larger Bay Area.
- Build a project consistent with the City's PDA designation by the ABAG and the MTC through the Bay Area's Regional FOCUS program, which was intended to encourage high density new development in close proximity to transit nodes that will help to reduce greenhouse gas emissions through a reduction in vehicle trips.
- Redevelop an underutilized property within the Specific Plan Area with a commercially viable, mixed-use development including Class A office, retail, high-density residential units and a hotel that enhances the hospitality facilities in Millbrae.
- Redevelop an underutilized property within the Specific Plan Area with mixed-use development, including a hotel, within a half-mile of the City's primary gateway near Highway 101 for direct access to the San Francisco International Airport and adjacent to the Millbrae Station to provide convenient access for residents, guests and employees to utilize public transit.
- Design and construct a project that accommodates the needs of transit service providers to ensure safe and reliable transit access continues.
- Provide clear and direct pedestrian and bicycle linkages along landscaped and lighted pathways throughout the mixed-use development with direct access to pedestrian and bicycle routes for residents, guests and employees to utilize multiple modes of transportation.
- Design and construct a high-quality, mixed-use development that minimizes the impact of new development on the character of surrounding residential neighborhoods and adds to the visual character of Millbrae Avenue.
- Activate public open spaces and streets by fronting them with pedestrian-friendly ground floor design and active retail uses.
- Utilizes design features at the corner of Millbrae Avenue and Rollins Road to help establish a gateway to Millbrae.
- Design and construct a project that provides publicly accessible open spaces.
- Achieve sustainable aspects of construction through current green building practices.

3.4.3 BUILDOUT OF THE TOD #2 PROJECT

As shown above in Table 3-1 in Section 3.1.4, Buildout Projections, the construction of the proposed TOD #2 project would result in the following:

- 164,535 square feet of office space
- 46,935 square feet of retail space
- 321 residential units
- 116 hotel rooms
- 851 residents
- 868 employees

PROJECT DESCRIPTION

3.4.4 PROJECT COMPONENTS OF TOD #2 PROJECT

The proposed TOD #2 project includes five key components described by the name of the sub-area of the 1998 Specific Plan (i.e. Sites 5A, 5B, 6A, 6B and 8). Figure 3-27 shows all five of the key component locations and Figure 3-28, shows the ground level for each of the proposed buildings. The following provides a summary of each of the project components by site number:

- Site 5A, located immediately south of the BART/Caltrain parking garage and drop-off area, would be developed with office and retail uses. A transit station plaza would be located between Site 5A and the Millbrae Station.
- Site 5B, located south of Site 5A, would be developed with residential and retail uses. A transit station plaza and pedestrian and bicycle paseo would run east-west between Sites 5A and 5B.
- Site 6A, located east of Site 5A and the BART/Caltrain parking garage west of Aviador Avenue, would be developed with hotel and retail uses. A large portion of this site would remain as surface parking that would be reconfigured as part of the project.
- Site 6B, located east of Site 5B and south of Site 6A, would be developed with office and retail uses.
- Site 8, located north of Highline Canal, would be paved for use as a surface parking lot for users of the Millbrae Station.

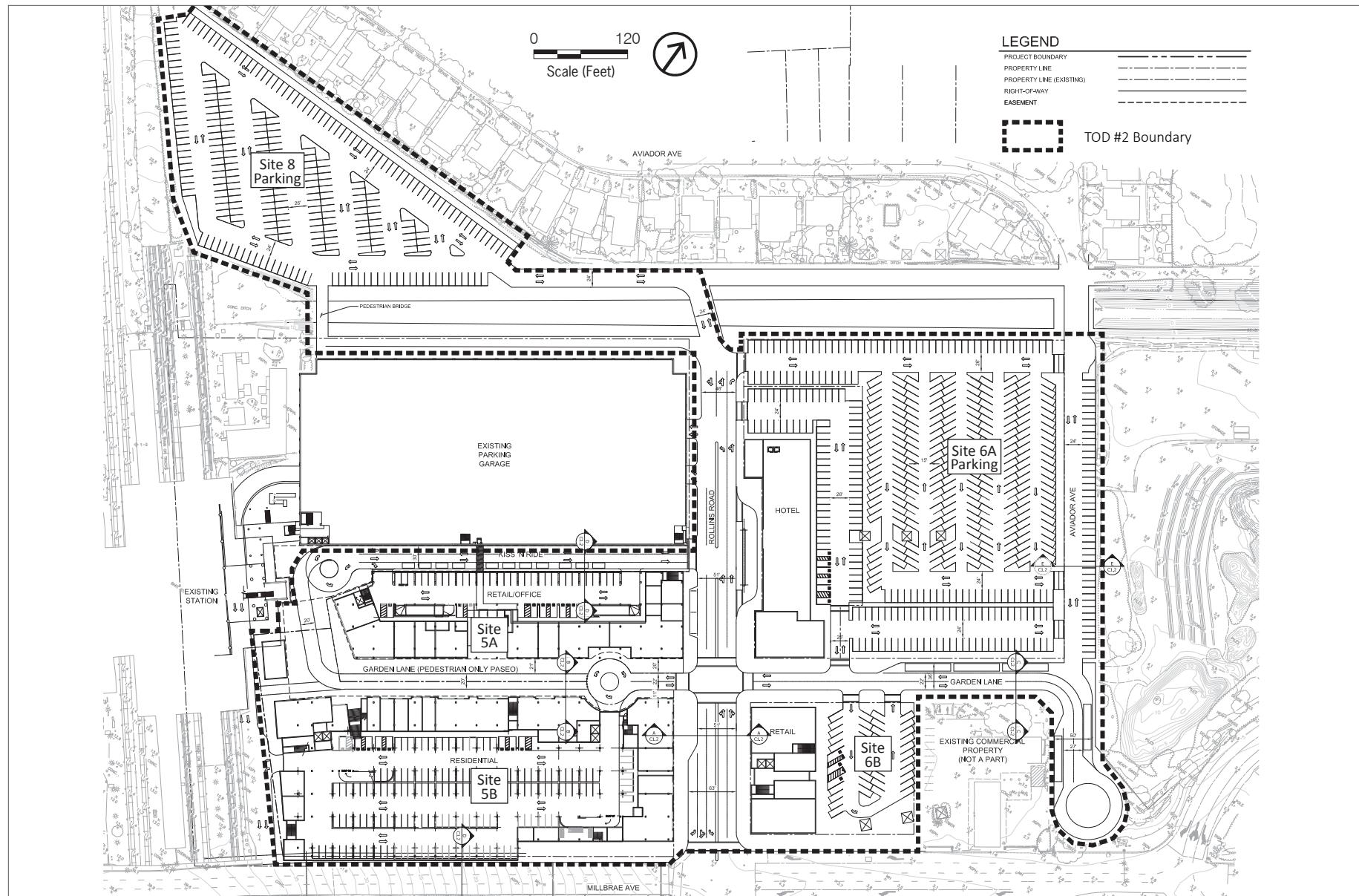
3.4.4.1 IMAGE AND CHARACTER

Building Design and Height

Section drawings for Sites 5A and 5B are shown in Figure 3-29 and building elevations are shown on Figures 3-30 through 3-33. The design standards and building plans for each component of the project are as follows:

- Site 5A would have a development area of 61,638 square feet, a FAR of 2.98, and a total building area of 183,690 square feet. The building would setback zero feet from Rollins Road and the BART drop-off, 14 to 25 feet from Garden Lane, and 0 to 20 feet from the new station plaza. The maximum height proposed would be up to 99 feet.
- Site 5B would have a development area of 124,426 square feet, a FAR of 4.65, and a total building area of 360,835 square feet. The building would be setback zero feet from Rollins Road and Garden Lane, 4 to 8 feet from South Station Road, 2 feet from Millbrae Avenue below the roadway profile, and 25 feet at and above the roadway profile. The maximum height would be up to 89 feet.
- Site 6A would have a development area of 156,112 square feet, a FAR of 0.41, and a total building area of 63,400 square feet. The building would be setback zero feet from Garden Lane and 3 to 16 feet from Rollins Road. The maximum height would be up to 55 feet.
- Site 6B would have a development area of 26,138 square feet, an FAR of 2.02, and a total building area of 52,800 square feet. The building would be setback zero feet from Rollins Road and Millbrae Avenue and 4 feet from Garden Lane. The maximum height would be up to 70 feet.

As previously stated in Section 3.2.1.4, San Francisco International Airport Land Use Compatibility Plan, height limits are subject to the SFO ALUCP, and FAA and SFO approval.

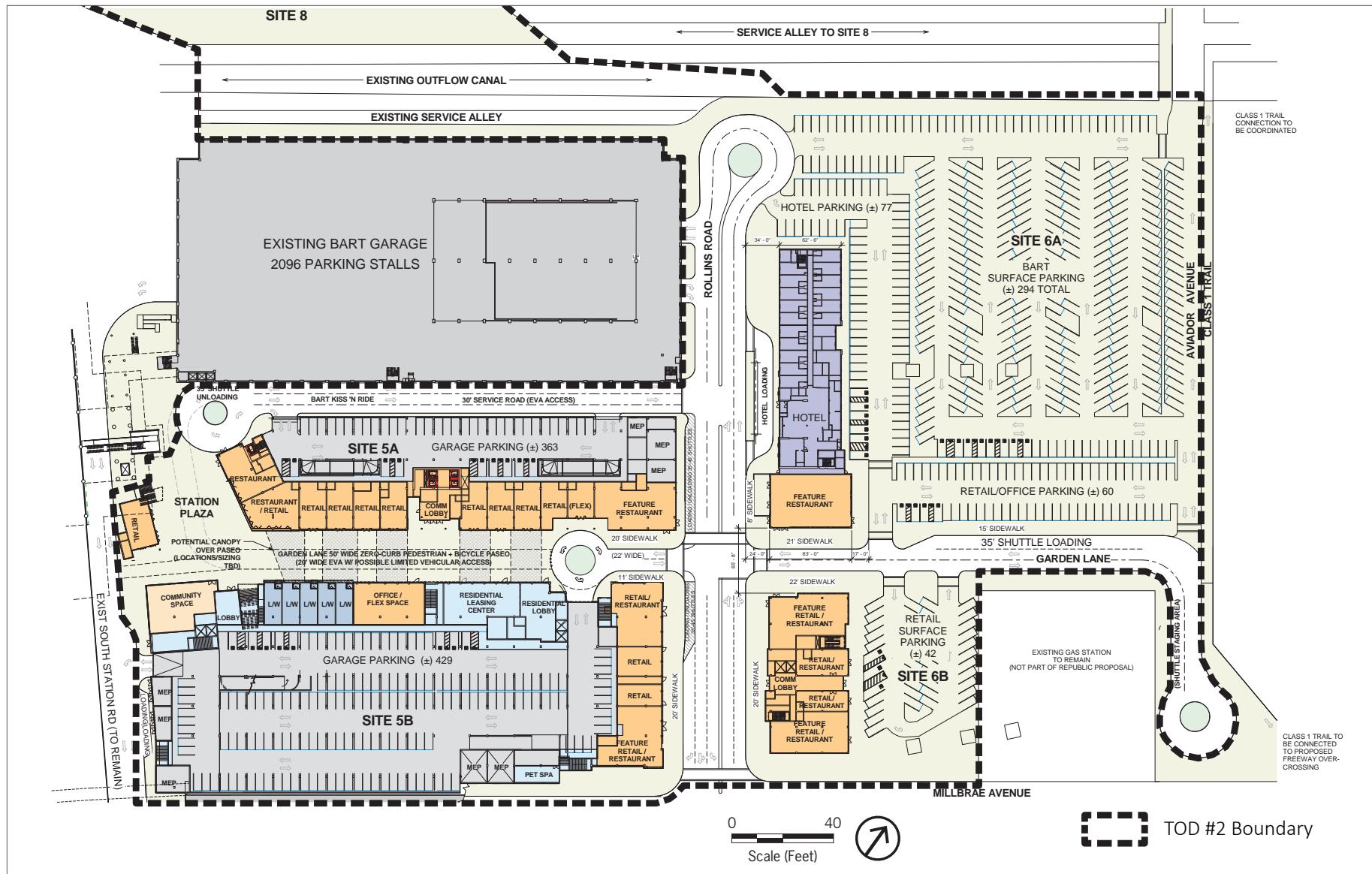


Source: Republic Millbrae LLC, September 11, 2014.

Figure 3-27

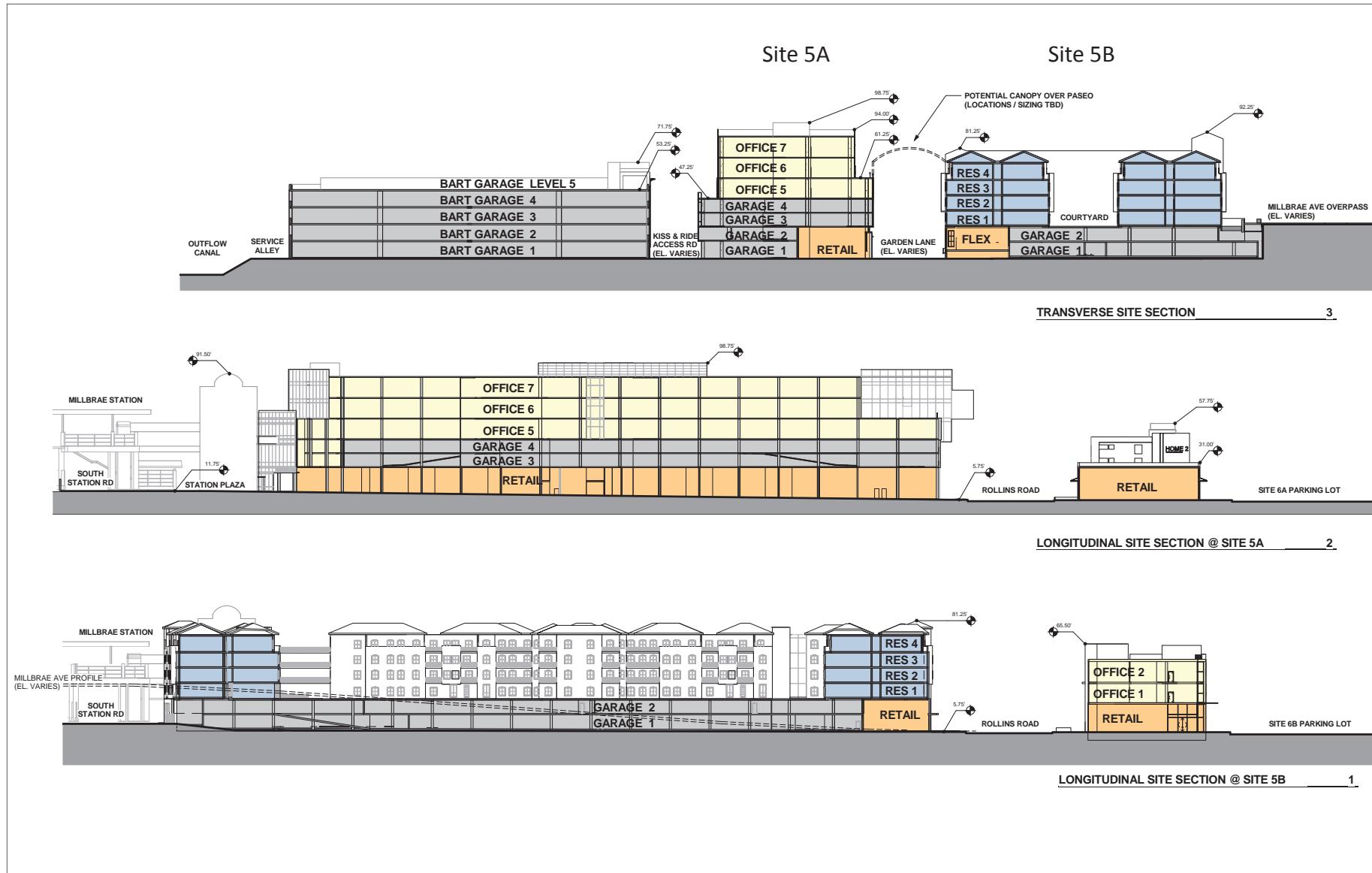
TOD #2 Site Plan

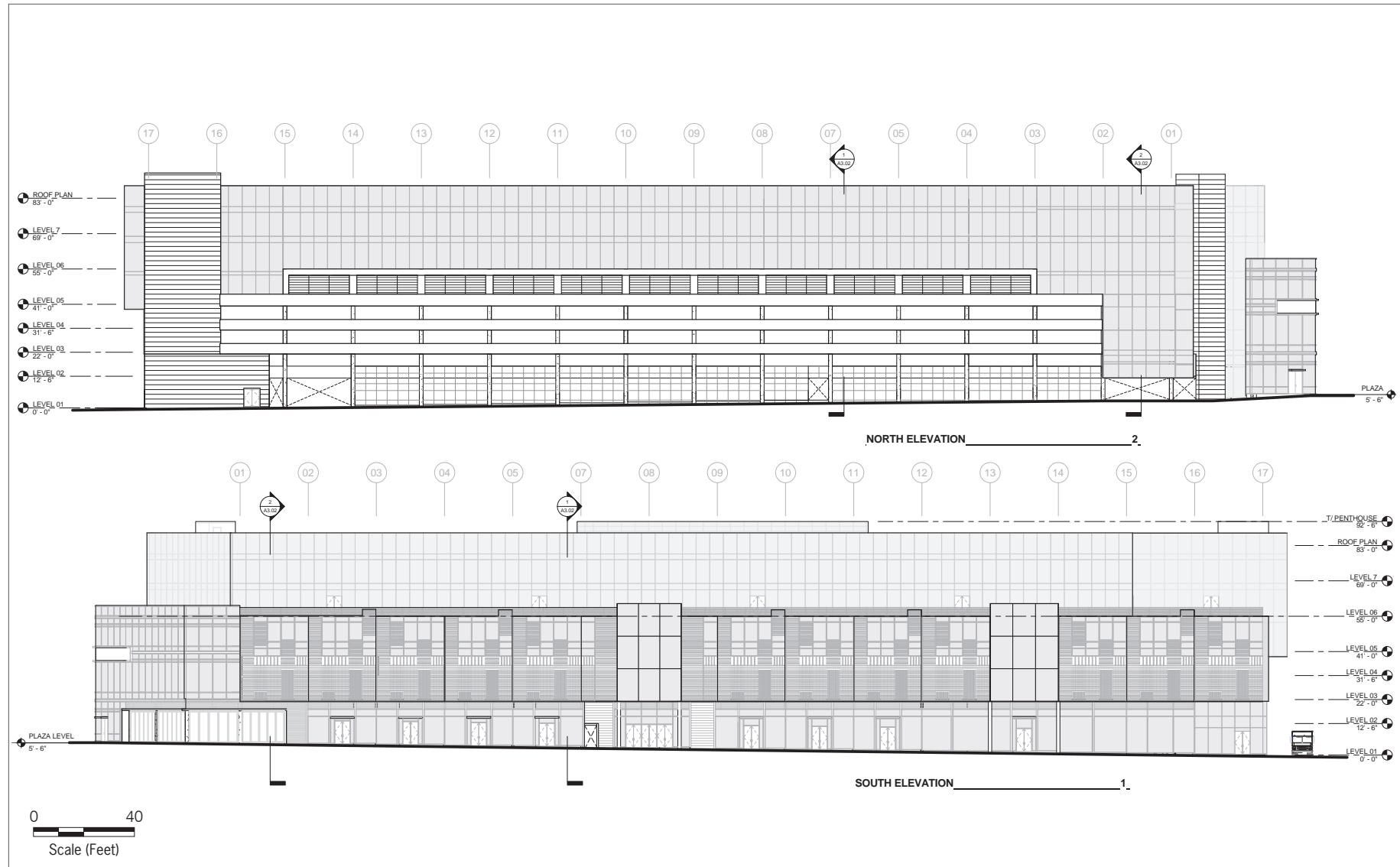
PROJECT DESCRIPTION



Source: Republic Millbrae LLC, September 29, 2014.

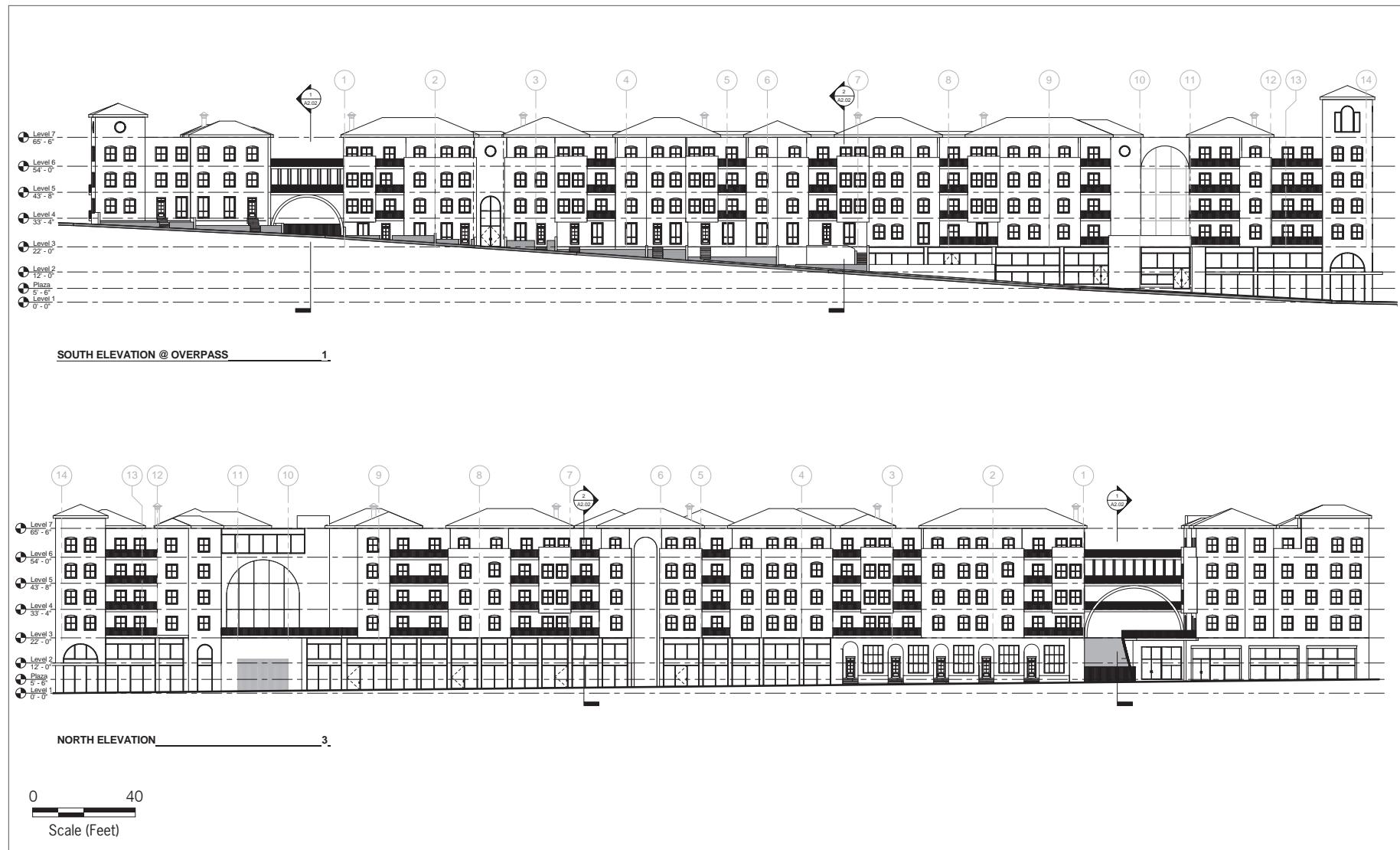
Figure 3-28
TOD #2 Site Plan at Ground Level

PROJECT DESCRIPTION

 Figure 3-29
TOD #2 Building Sections



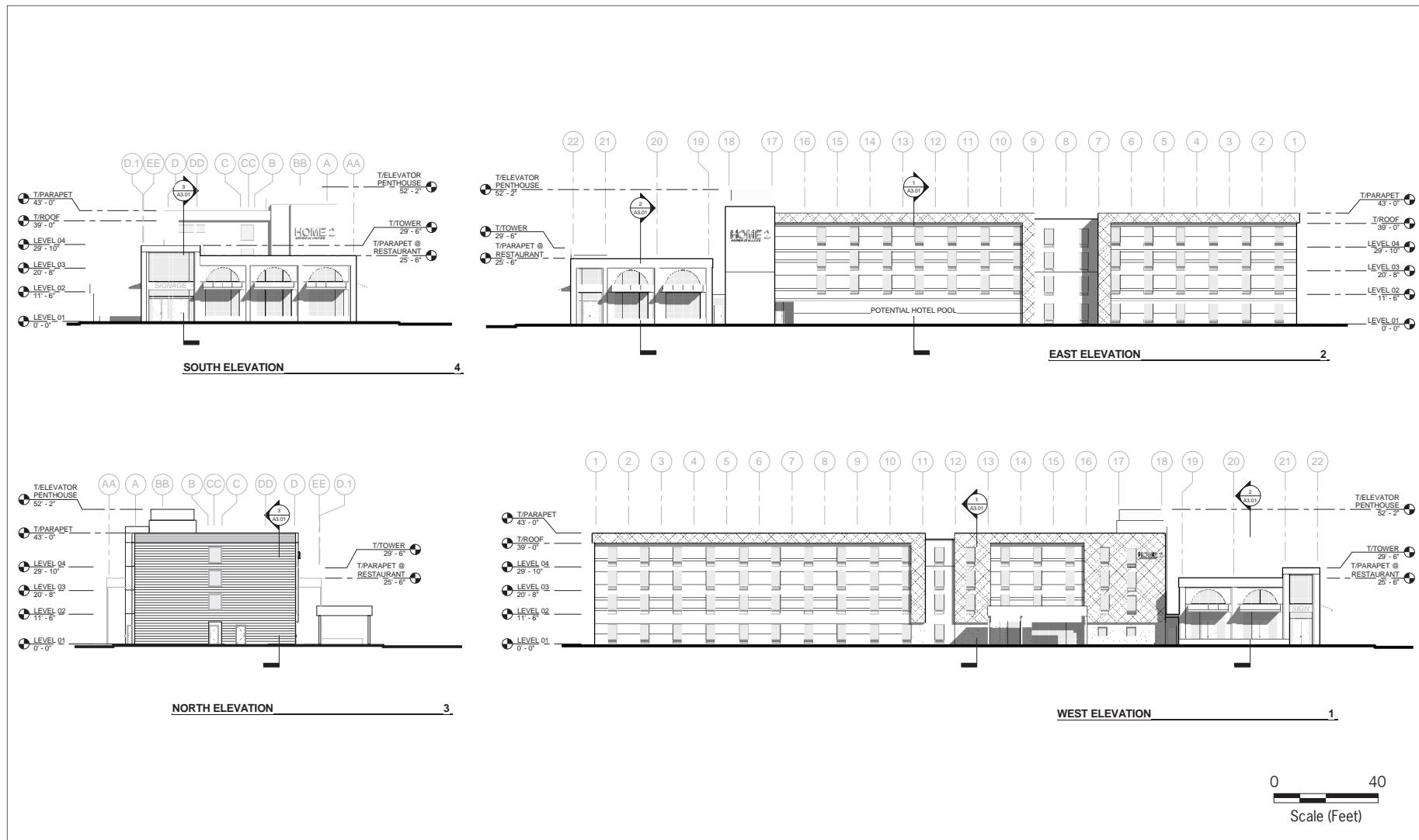
Source: Republic Millbrae LLC, September 11, 2014.

Figure 3-30
TOD #2 Building Elevations Site 5A

PROJECT DESCRIPTION


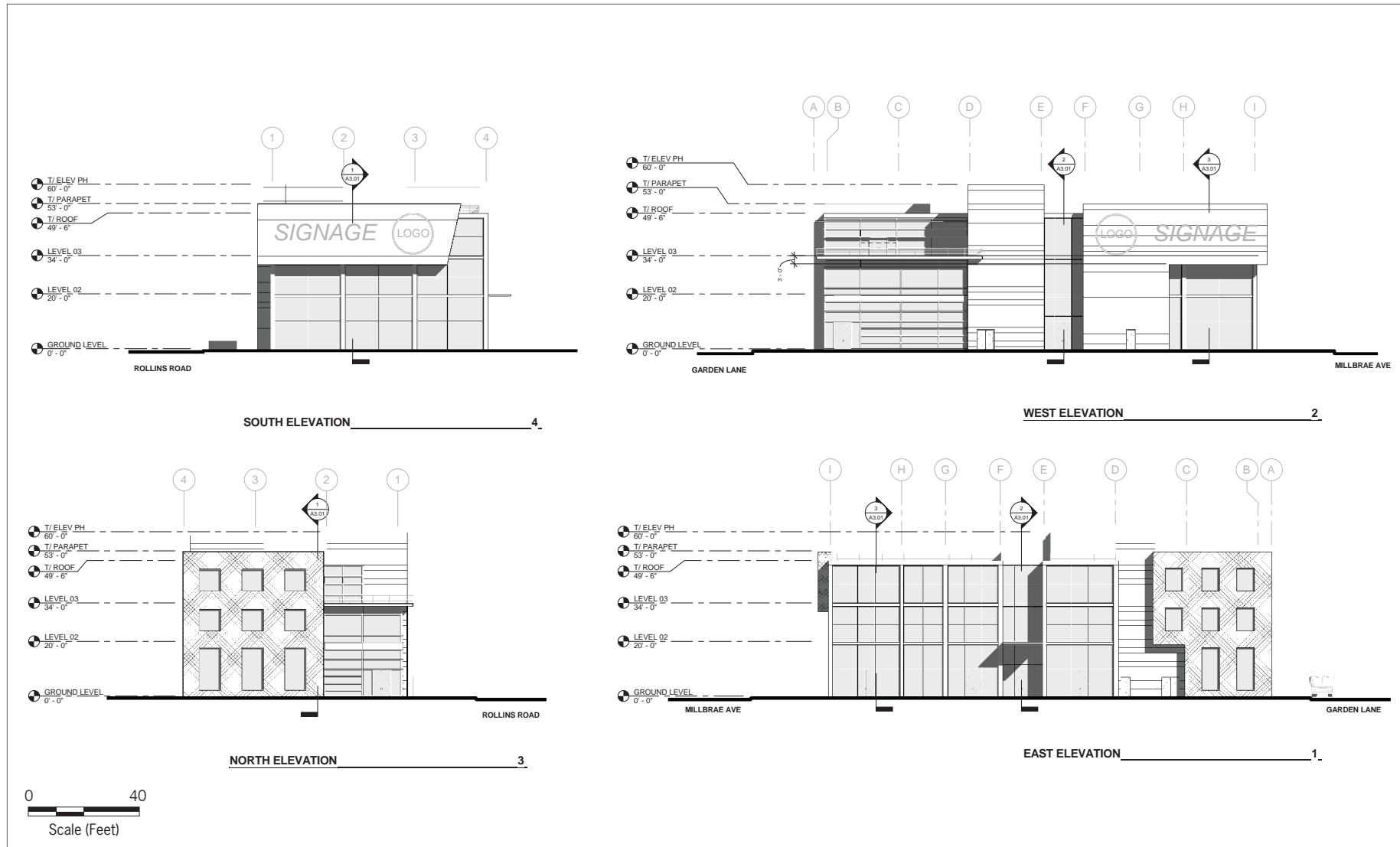
Source: Republic Millbrae LLC, September 11, 2014.

Figure 3-31
TOD #2 Building Elevations Site 5B

PROJECT DESCRIPTION


Source: Republic Millbrae LLC, September 11, 2014.

Figure 3-32
TOD #2 Building Elevations Site 6A

PROJECT DESCRIPTION


Source: Republic Millbrae LLC, September 11, 2014.

 Figure 3-33
 TOD #2 Building Elevations Site 6B

PROJECT DESCRIPTION

Streetscape and Landscape Design

The project would install streetscape features along the project site perimeter and the linear bicycle and pedestrian paseo, including street lights, sidewalk treatments, trees, planters, landscaping, and street furniture. Landscaping features are illustrated on Figure 3-34. The proposed TOD #2 project would provide landscaping and surface treatment at the Millbrae Station plaza and new Garden Lane. Street trees would be provided in the streets within the TOD #2 project site, as well as in surface parking areas. Sidewalks would have street trees and plantings. A linear park would be created along the north side of Highline Canal, between the crossing to Site 8 and Aviador Road. As shown on the proposed landscaping for the TOD #2, plants would be grouped with similar water, climatic and soil requirements to conserve water and create a drought responsive landscape. As many as 200 trees, including street trees, would be planted throughout the project site. The street tree types will be determined according to the Millbrae Municipal Code Chapter 8.60, Millbrae Tree Protection and Urban Forestry Program during the City's design review process.²⁸

Lighting

The source, intensity, and type of exterior lighting for the project site would be typical for orientation and safety needs. All on-site lighting would be low-level illumination, downward facing and shielded to reduce light spillover or glare. In landscaped and paved areas, light sources will be concealed. All exterior surface and above-ground mounted fixtures will be sympathetic and complementary to the architectural theme.

3.4.4.2 OFFICE

The proposed project includes a total of 164,535 square feet of Class A²⁹ office space on Sites 5A, 5B and 6B as follows:

- Site 5A would be developed with approximately 123,600 675 square feet of Class A office space. Offices would be located on the fifth, sixth, and seventh stories of the building on Site 5A, and would be served by two levels of parking on the first, second, and third stories.
- Site 5B would be developed with 3,000 square feet of incubator office space that would be located on the ground floor and would be served by the on-site parking garage.
- Site 6B would be developed with 25,000 square feet of office space that would be located on the second and third stories that would be served by an on-site parking lot.

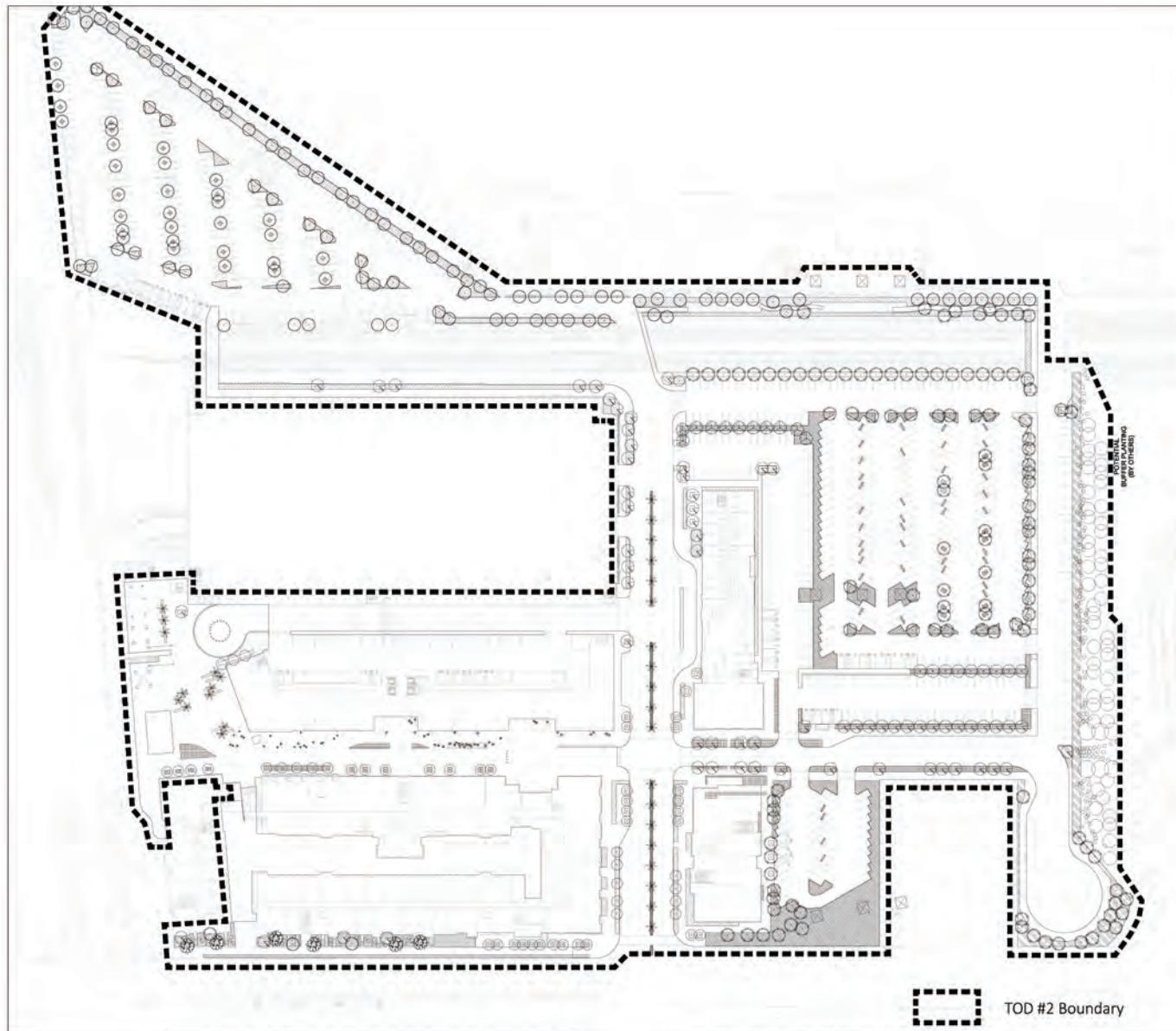
Offices uses are anticipated to operate during normal business hours (e.g. 8:00 am to 5:00 pm) with limited after-hours access for cleaning/maintenance crews. Based on an average of 1 job per 250 square feet for office, the proposed project would generate up to 658 employees.

²⁸ Millbrae Municipal Code Title 8 (Public Works), Chapter 8 (Millbrae Tree Protection and Urban Forestry Program).

²⁹ These buildings represent the highest quality buildings in their market. They are generally the best looking buildings with the best construction, and possess high quality building infrastructure. Class A buildings also are well-located, have good access, and are professionally managed. As a result, they attract the highest quality tenants and also command the highest rents.



PROJECT DESCRIPTION



Source: Republic Millbrae LLC, September 11, 2014.

0 120
Scale (Feet)

Figure 3-34
TOD #2 Landscape Plan

PROJECT DESCRIPTION

3.4.4.3 RETAIL

The proposed project includes a total of 46,935 square feet of retail space that could include a mix of small-format destination retail (e.g. drug store, restaurant), neighborhood-serving retail (e.g. dry cleaning, mailing/shipping service) on Sites 5A, 5B, 6A and 6B as follows:

- Site 5A would be developed with approximately 18,200 square feet of retail and restaurant space, most of which would be located on the first floor and served by parking on the first floor. A fitness center would be located on the third floor.
- Site 5B would be developed with 11,100 square feet of retail and restaurant space, of which 9,600 square feet would be on the ground floor of the main building and 1,500 square feet would be in a stand-alone “iconic building.”³⁰ The iconic building would contain a ground floor restaurant with rooftop bar.
- Site 6A would be developed with 5,000 square feet of ground floor restaurant space with outdoor patio dining.
- Site 6B would be developed with 10,400 square feet of ground floor retail and restaurant space.

Based on an average of 1 job per 400 square feet for retail, the proposed project would generate up to 117 employees.

3.4.4.4 HOTEL

The proposed project would develop a four-story hotel with approximately 52,800 square feet of building area and up to 116 guest rooms on Site 6A. The first floor of the hotel would include the lobby, reception area, fitness room, pool, employee lounge, laundry room, offices, and electrical, mechanical, storage spaces, and some guest rooms. Additional guest rooms would be located on the first through fourth floors.

The hotel guest rooms would include a range of studio king bed rooms, studio double queen bed rooms, one-bedroom king bed rooms, one-bedroom double queen bed rooms, two-bedroom king bed rooms, and two-bedroom double queen bed rooms.

The hotel would be served by a dedicated on-site parking lot. The hotel is anticipated to operate 24 hours a day.

Based on an average of 1 job per 1.25 hotel rooms, the proposed project would generate up to 145 employees.

3.4.4.5 RESIDENTIAL

The proposed project would involve construction of up to 321 market-rate rental apartment units in one building with two towers as shown on Figure 3-29. The residential density would be 33 units per acre. The project would include 50 studio, 50 one-bedroom, 171 two-bedroom and 50 one-bedroom live/work apartment units. Based on

³⁰ Iconic buildings are a defining feature of a site or a larger area, in this case the Specific Plan Area. Iconic buildings may differ in height, shape, and function from the buildings that surround it. An iconic building typically has a unique design.

an average household size of 2.65 persons,³¹ it is assumed the proposed project would house approximately 851 residents. As about half of the proposed apartment units would be studio and one-bedroom units, it is likely that a resident population of 851 is high, thereby allowing for a conservative analysis of potential environmental impacts. It is anticipated that residents of the project would be drawn largely from Millbrae and other communities in the San Francisco Bay Area.

The residences would be located on the third through sixth stories and would be served by the on-site parking garage. The proposed apartment building would be oriented around a central courtyard where outdoor seating, lounge chairs and tables would be provided. Resident amenities such as a fitness center, indoor lounge, and leasing office would be provided. In total, common areas and resident amenities would comprise approximately 19,100 square feet of space (62 square feet per unit) and 3,000 square feet of community space located on the ground floor.

3.4.4.6 CIRCULATION AND ACCESS

Vehicular

Vehicular access and circulation is shown on Figure 3-28 above. The TOD #2 project site includes one main vehicular access point at the Rollins Road/Millbrae Avenue intersection. The project would improve Garden Lane that connects Rollins Road and Aviador Avenue. Site 5A would be accessed via the Service Road off of Rollins Road just south of the BART parking garage. Site 5B would be accessed via the improved Garden Lane extension to the west of Rollins Road between Site 5A and 5B. The hotel and restaurant on Site 6A and the retail development on 6B would be accessed by either Rollins Road or the new Garden Lane extension to the east. Site 8 would be accessed by the Aviador Avenue, which crosses over the Highland Canal and not via the portion of Aviador Avenue that runs through the adjacent residential neighborhood to the north of the project site.

The proposed TOD #2 project would include a station drop-off driveway and new station plaza at Millbrae Station. Bus access would also be provided within the TOD #2 project site, connecting to Rollins Road.

Emergency Response Vehicles

The proposed project would be required to comply with the minimum specifications in Municipal Code Chapter 9.30, which includes the California Fire Code, adopted by reference and local amendments.³² Emergency response vehicles would access the TOD #2 project site via Rollins Road from Millbrae Avenue. From this point, they can access the proposed TOD #2 project components from Rollins Road, Garden Lane, Aviador Avenue, and the new Kiss and Ride area located near the Millbrae Station (see Figure 3-28). The Garden Lane Paseo would also have emergency access from the Garden Lane roundabout, as well as the roundabout at the terminus of the Kiss and

³¹ Population is based on 2.65 persons per dwelling units consistent with U.S. Census Bureau's 2005-2009 American Community Survey 5 year estimates.

³² Millbrae Municipal Code, Title 9, Building Regulations, Chapter 9.30, Fire Code.

PROJECT DESCRIPTION

Ride area. No through access is granted at Aviador Drive north of the Highline Canal. Therefore, emergency response vehicles would not be able to access the TOD #2 project site from this point.

Pedestrian and Bicycle

As shown on previously on Figure 3-28, sidewalk width in the TOD #2 project site would vary from eight to 22 feet in width around the perimeter of the buildings. With the exceptions of the east-west sidewalk on the southwest corner and the north-south sidewalk on the northeast corner of Garden Lane and Rollins Road, these sidewalks vary between 20 and 22 feet in width. Pedestrian and bicycle access is also provided via the 50-foot wide Garden Lane paseo.

As shown in Figure 3-35, the proposed TOD #2 project would provide bicycle access along the new Rollins Road, Garden Lane, and Aviador Avenue. A Class I bicycle facility would be provided on Aviador Avenue and would connect to the planned Bay Trail. A Class III bicycle facility would also be included along Aviador Avenue and the new Garden Lane. Additional bicycle connections would be included on Aviador Avenue and the new Garden Lane, as well as the Garden Lane paseo and the north side of Millbrae Avenue west of Rollins Road. Pedestrian connections will lead from Millbrae Avenue, the Site 6A parking lot, and the Site 8 parking lot, to the Station Plaza. Retrofitted bicycle parking will be included in the current station parking structure; Staple style bike racks will be included at the western terminus of the Garden Lane paseo; Secure bike parking would be located in the Site 5A and Site 5B parking garages; BART bike lockers would be placed at the station plaza; and bike lockers would be placed in the Site 5B parking garage.

3.4.4.7 PARKING

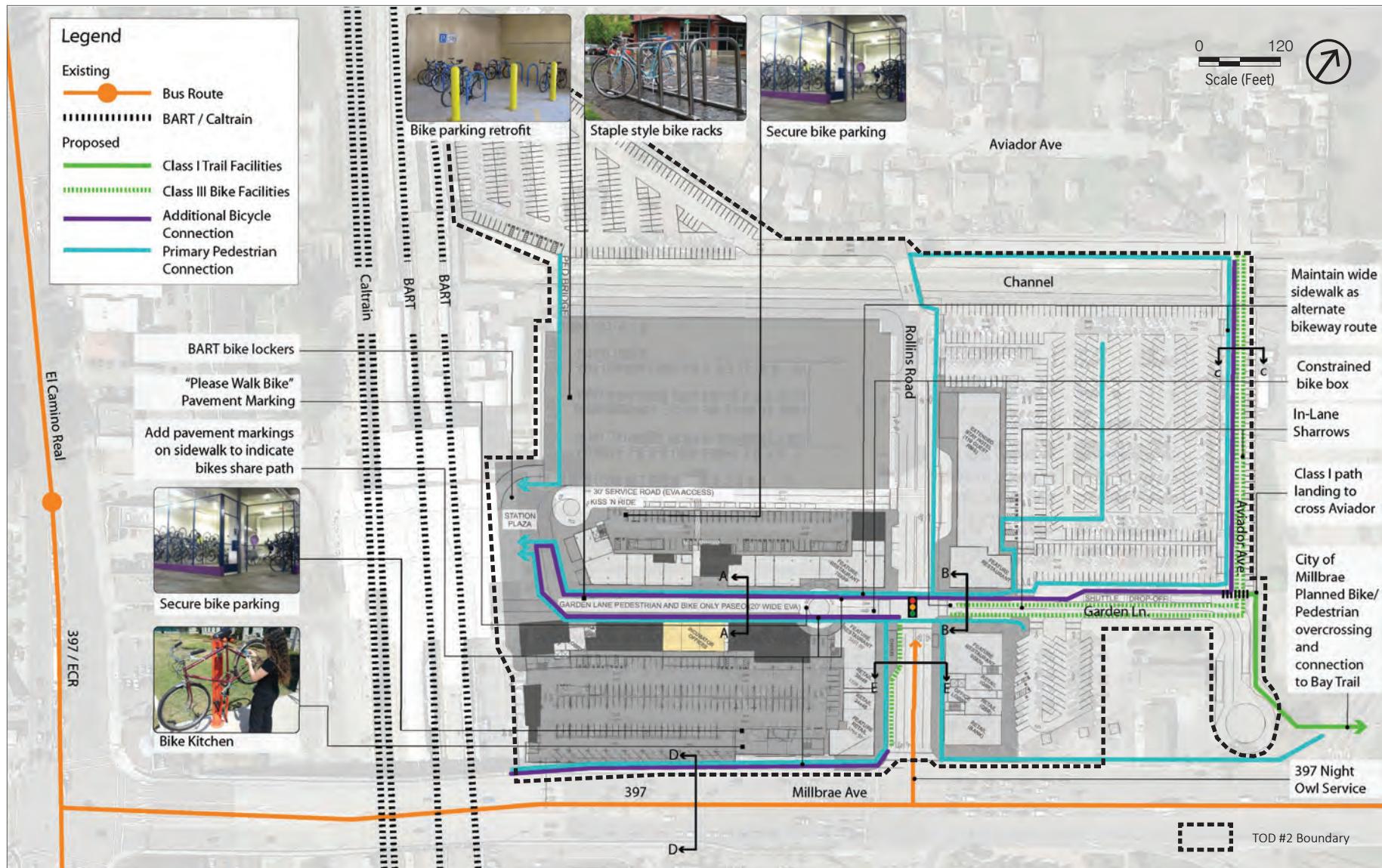
The proposed parking plan is shown in Figure 3-36. Parking on the TOD #2 project site would be provided as follows:

- Parking on Site 5A would be located on the first through fourth stories.
- Parking on Site 5B would be located on the ground floor, second story, and in one level of underground parking.
- Parking on Site 6A would be located in a surface parking lot.
- Parking on Site 6B would be located on Site 6A.
- Site 8 would provide 292 parking spaces for Millbrae Station users. Parking would be located in a surface parking lot.

As further explained in Chapter 4.13, Transportation and Circulation, of the Draft EIR, the proposed TOD #2 project would eliminate all 883 surface level BART parking lot spaces and provide 317 replacement BART parking spaces, for a total net reduction of 566 parking spaces.

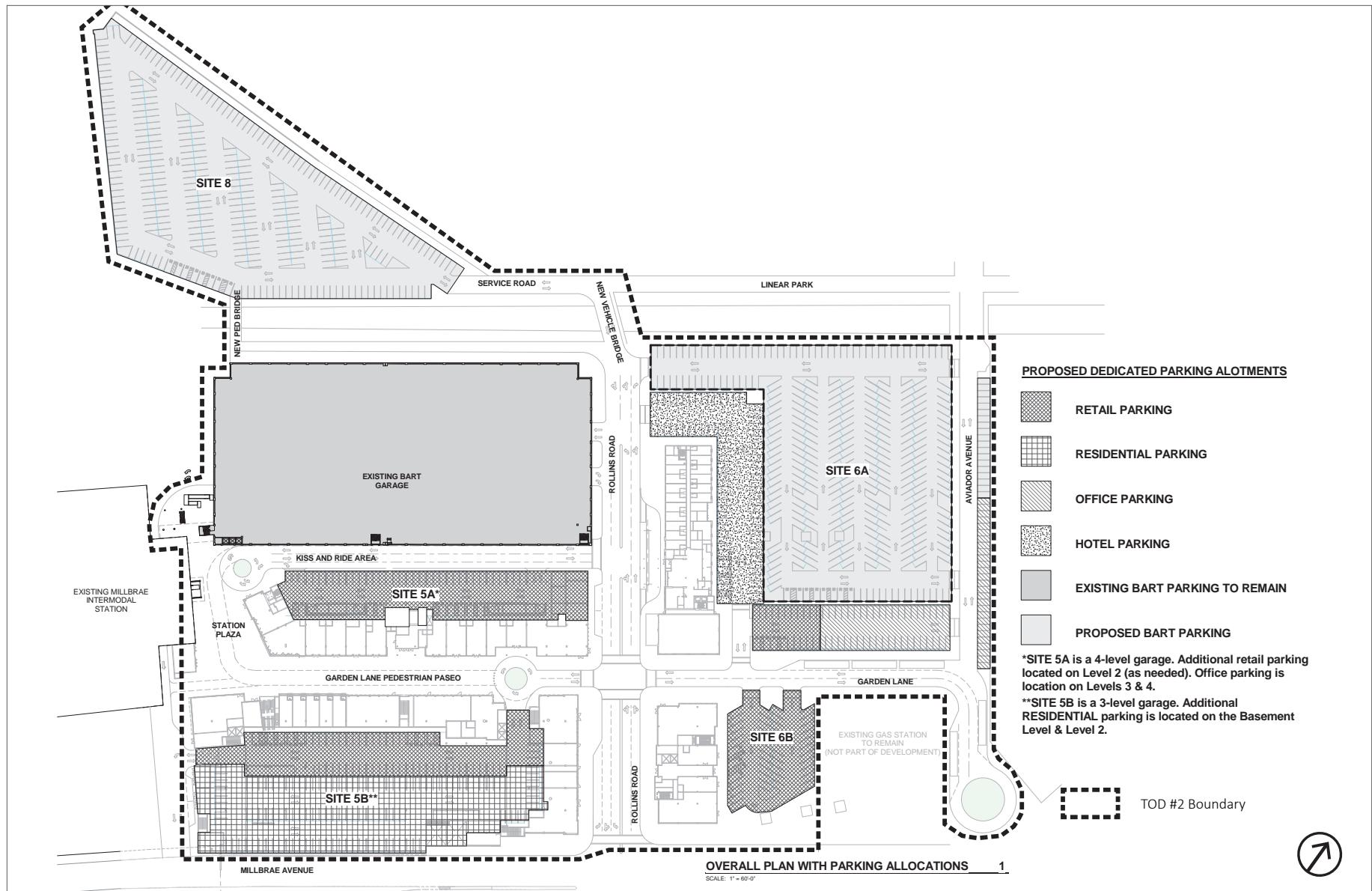


PROJECT DESCRIPTION



Source: Republic Millbrae LLC, September 11, 2014.

Figure 3-35
TOD #2 Pedestrian and Bicycle Plan

PROJECT DESCRIPTION


Source: Republic Millbrae LLC, September 11, 2014.

Figure 3-36
TOD #2 Parking Plan

PROJECT DESCRIPTION

Table 3-10 shows the total parking stalls by land use for vehicles and bicycles. Per the Specific Plan Update, parking for both short-term and long-term bicycle parking would be required as described in Table 3-6 in Section 3.2.3.3 Development Standards above.

TABLE 3-10 TOD #2 PROJECT PARKING SUMMARY

Land Use	Size	Vehicular Parking Stalls	Long-term Bicycle Parking Stalls	Short-term Bicycle Parking Stalls
Office	164,535 sf	372	17	9
Retail	46,935 sf	180	4	24
Hotel	116 rooms	79	0	0
Residential	321 units	372	246	25
Surface Parking	-	609	-	-
Total		1,612	267	58

Notes: TOD = transit-oriented development, sf = square feet

Source: Republic Millbrae LLC, Proposed Millbrae Transit Oriented Development Site Development Application, September 11, 2014.

3.4.5 SITE PREPARATION AND CONSTRUCTION

3.4.5.1 DEMOLITION, SITE PREPARATION AND GRADING

The Applicant proposes to demolish the existing parking lots that are currently on the project site.³³ As the construction of the proposed TOD #2 project would occur over an approximate five-year period in four separate phases for each of the four proposed project components (i.e. Site 5A, 5B, 6A, and 6B and Site 8), demolition, site preparation, and grading would take place at various times over the course of the project. The demolition time period for each construction phase would take place over a period of approximately one month, beginning as early as January 2016. Site preparation and grading would be completed over a three-month period during the second phase. Demolition debris would be off-hauled for disposal in accordance with the Municipal Code Section 9.35.010.³⁴ Debris to be hauled would include approximately 1,900 square feet of building demolition debris and 3,370 tons of asphalt/concrete material. No soil off-haul would occur as any disturbed soil will be balanced on-site. Typical equipment to be used for demolition, grading, and trenching would include backhoes, excavators,

³³ The demolition and construction details in this EIR are calculated estimates based on the project's preliminary site plans and are used for environmental review purposes only. The exact time frames and construction equipment will be determined as the project fully matures during the project approval process.

³⁴ Millbrae Municipal Code, Title 9, Building Regulations, Chapter 9.35, California Green Building Code, Section 9.35.010 Adoption of the California Green Building Code Title 24, Part 6, 2013 Edition.

PROJECT DESCRIPTION

concrete saws, skid steer loaders, graders, rubber-tired dozers, scrapers, and water trucks. During the building construction phases, which would occur over a two-year period, The project construction would be comprised of 321 residential units, 46,935 square feet of retail space including a 5,250 square foot restaurant, 164,535 square feet of office space, an approximately 351,251 square foot parking garage, 246,000 square feet of paved parking lots and 3.68 acres of hardscape (e.g. roadways, curb, gutters, planters, seat walls, etc.). Pile driving would occur for approximately two months during the building construction phase of each building. Typical equipment to be used for building construction of the project would include forklifts, backhoes, a crane, loaders, aerial lifts, a generator, welders, cement mixers, rollers, pavers, and air compressors.

3.4.5.2 UTILITIES

Water Supply and Conservation

The proposed TOD #2 project would provide new water lines that would tie into the existing 12-inch water line along Highline Canal, 8-inch line along the proposed new Garden Lane, and 8-inch line along Aviador Avenue.

The project incorporates a number of features meant to conserve water used for on-site irrigation. Water conserving features include an automatic “smart” irrigation controller with rain-sensor, low precipitation/low angle irrigation spray heads, low volume drip tubing installed below mulch, low water consuming plants, soil moisture retention techniques, and mulching to reduce evapotranspiration from the root zone. As previously described and shown on Figure 3-35, the project’s landscaping would be grouped with similar water, climatic and soil requirements to conserve water. In addition, landscaping would include native and drought resistant plants.

Sewer Service

The proposed TOD #2 project would provide new sanitary sewer lines that would tie into the existing 18-inch sanitary sewer line along Highline Canal and 8-inch line along Aviador Avenue. Stormwater drainage would tie into the existing 36-inch Highline Canal facility and 15-inch line along the proposed new Garden Lane.

Solid Waste Service Access

Front load dumpster bins would be stored in the trash rooms of each of the buildings. Trash chutes would be provided on each floor. Dumpsters would be transported by lift to the trash enclosure location for pick up by the waste management company on trash day.

Electricity and Natural Gas

Electricity and gas are provided to the project site by PG&E. The existing electrical system consists of overhead and underground facilities. New underground electrical lines would be required to replace existing lines if realignment is required under future development.

Stormwater

There is an existing storm drain network within the area, including a 36-inch line that runs under the Highline Canal, a 15-inch storm drain beneath Garden Lane pedestrian paseo, and a 15-inch storm drain beneath Aviador Avenue. Proposed development within the TOD #2 project site includes construction of a new internal storm drain network and the installation of various biotreatment areas and storm drain manhole media filter inserts. Numerous biotreatment areas would be located within the proposed parking lots and at the cul-de-sac at the end of Aviador Avenue with new storm drains connecting these facilities to the existing storm drain system. See Figure 4.8-4 in Chapter 4.8, Hydrology and Water Quality, of this Draft EIR.

3.4.6 PUBLIC SERVICES

The same public service providers described in Section 3.2.7, Public Services, above will provide services to the proposed TOD #2 project.

3.4.7 REQUIRED APPROVALS

The proposed TOD #2 project would require the following approvals from the City:

- General Plan and Zoning Ordinance Amendment
- Conditional Use Permit
- Development Agreement
- Demolition, Grading and Building permits

This project will also require a number of approvals from other agencies, including, but not limited to:

- San Francisco Regional Water Quality Control Board
- City/County of Association of Governments
- Federal Aviation Administration
- Caltrain
- San Francisco Bay Area Rapid Transit

PROJECT DESCRIPTION

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